

Crossfield Downtown and Entrance Area Redevelopment Plan



September 27, 2010

Bylaw No. 2010-11



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Town of Crossfield

Bylaw No. 2010-11

Being a Bylaw to Adopt an area redevelopment plan for the Town of Crossfield, in the Province of Alberta pursuant to Sections 634 and 635 of the Municipal Government Act, Statutes of Alberta, 1994, Chapter M26.1, and amendments thereto, the Town of Crossfield Council may, by bylaw, adopt a plan for the municipality to be known as the Town of Crossfield Downtown and Entrance Area Redevelopment Plan;

WHEREAS

pursuant to Section 635 of the same Act, an area redevelopment plan shall describe the land uses proposed for the municipality; the manner of and the proposals for future development in the municipality; the objectives of the plan and how they are proposed to be achieved; the proposed land uses for the redevelopment area; if a redevelopment levy is to be imposed, the reasons for imposing it, and any proposals for the acquisition of land for any municipal use, school facilities, parks and recreation facilities or any other purposes the council considers necessary;

AND WHEREAS

pursuant to Section 634 of the same Act, an area redevelopment plan may designate an area of the municipality as a redevelopment area for the purposes of preserving or improving land and buildings in the area; rehabilitating buildings in the area; removing buildings from the area; constructing or replacing buildings in the area; establishing, improving or relocating roads, public utilities or other services in the area; facilitating any other development in the area;

AND WHEREAS

the Municipal Council for the Town of Crossfield in the Province of Alberta (hereinafter called the Council") in a duly assembled meeting did pass a resolution authorising the preparation of an area redevelopment plan;

AND WHEREAS

the Council has caused to be prepared by its Administration a draft Downtown and Entrance Area Redevelopment Plan for Council approval;

NOW THEREFORE the Council, duly assembled, hereby enacts as follows:

1. This Bylaw may be cited as the "Downtown and Entrance Area Redevelopment Plan";
2. The Downtown and Entrance Area Redevelopment Plan, being Schedule "A", as attached to and forming a part of this Bylaw is hereby adopted.
3. This Bylaw comes into full force and effect upon the date of third and final reading.

Read a first time September 7, 2010

Read second time September 21, 2010

Read a third time September 21, 2010

1.0 ARP Purpose, Vision and Goals

1.1 ARP Purpose

In accordance with the Province of Alberta Municipal Government Act (MGA), the purpose of the Downtown and Entrance Area Redevelopment Plan (ARP) is to provide policies for the future redevelopment of the Downtown and Entrance Area of the Town of Crossfield, as illustrated in Figure 1 - Downtown and Entrance Redevelopment Area. The policies of this ARP include land use policies, architectural and design guidelines, and a public improvement program to enhance public lands and rights-of-way within the Downtown and Entrance Area.

This ARP is a long-range policy document intended to guide the Town of Crossfield Council, the Town of Crossfield administration, government agencies, public and private utilities and service providers, other local authorities such as the Rocky View School Division and Rocky View County, non-governmental organisations, developers, local businesses, and residents in making sustainable, responsible, and accountable decisions with respect to land use, development, service provision, and public improvements within the Downtown and Entrance Area. The ARP sets forth objectives and policies concerning:

- Environmental Stewardship and Sustainability
- Economic Development
- Land Use and Development
- Architectural and Design Guidelines
- Transportation
- Servicing and Utilities
- Public Improvement Program

This ARP conforms with the Town of Crossfield Municipal Development Plan (MDP), which is the primary planning document for the Town of Crossfield. The policies of this ARP shall be achieved through amendments to the Town of Crossfield Land Use Bylaw and implementation of the proposed public

improvement program. All future land use plans, policies, guidelines, planning approvals, private developments, and public improvements within the Downtown and Entrance Area must conform to the objectives and policies of this ARP.

The Downtown and Entrance ARP is not a static document, but will evolve and adapt to changing circumstances in order to accommodate new trends and to promote innovative initiatives. In addition to addressing the ongoing administration, monitoring, and implementation of the ARP objectives and policies, Part 3 of this plan provides for a public improvement program to enhance public lands and rights-of-way within the Downtown and Entrance Area.

This ARP is established under the authority of Sections 634 and 635 of the MGA, which address the adoption of Area Redevelopment Plans and stipulate which matters must and may be addressed by ARPs:

- 634** A council may
- a) designate an area of the municipality as a redevelopment area for the purpose of any or all of the following:
 - (i) preserving or improving land and buildings in the area;
 - (ii) rehabilitating buildings in the area;
 - (iii) removing buildings from the area;
 - (iv) constructing or replacing buildings in the area;
 - (v) establishing, improving or relocating roads, public utilities or other services in the area;
 - (vi) facilitating any other development in the area,
 - b) adopt, by bylaw, an area redevelopment plan,
 - c) in accordance with this section and Division 6, provide for the imposition and collection of a levy to be known as a “redevelopment levy”, and

- d) authorize a designated officer, with or without conditions, to perform any function with respect to the imposition and collection of that redevelopment levy.

1995 c24 s95

635 An area redevelopment plan

- a) must describe
 - (i) the objectives of the plan and how they are proposed to be achieved,
 - (ii) the proposed land uses for the redevelopment area,
 - (iii) if a redevelopment levy is to be imposed, the reasons for imposing it, and
 - (iv) any proposals for the acquisition of land for any municipal use, school facilities, parks and recreation facilities or any other purposes the council considers necessary,
 and
- b) may contain any other proposals that the council considers necessary.

1995 c24 s95

This ARP has been adopted by the Town of Crossfield Council through a Public Hearing and approval of a statutory bylaw. The Downtown and Entrance Area ARP conforms with the Town of Crossfield MDP and the requirements of the MGA in all respects.

1.2 Sustainability Plan Vision and Goal for Downtown Crossfield

On December 15, 2009, the Town of Crossfield Council approved the Crossfield Sustainability Plan. That Council approval marked the beginning of an ongoing process of engagement in continually making Crossfield a more sustainable community. Successful implementation of the Crossfield Sustainability Plan will depend upon the future commitment of a number of champions in the community. Those champions have been identified in the Crossfield Sustainability Advisory Board, among residents of the town, throughout the business community, in the local schools, and in the Town Council and administration.

One of the top priorities of the Crossfield Sustainability Plan is the revitalisation of downtown Crossfield. More specifically, the Crossfield Sustainability Plan calls for the adoption of a Downtown and Entrance Area Redevelopment Plan to provide a framework for downtown beautification through adherence to both architectural and design guidelines and a public improvement program.

The Crossfield Sustainability Plan Vision for the Downtown and Entrance Area foresees a Vibrant Main Street:

Railway Street is the vibrant commercial heart of Crossfield. Residents and visitors enjoy convenient pedestrian access to a full range of services and amenities including coffee shops, restaurants, outdoor patios, and trendy artisan stores and boutiques. The architecture along Railway Street reflects our pioneer heritage with local and natural façades of wood, sandstone, and brick, while appropriately integrating contemporary design. The Town's Downtown and Entrance Area Redevelopment Plan, in conjunction with the Land Use Bylaw, ensures conformity with high architectural standards. Attractive landscaping and buildings are developed at a human scale that enhances the pedestrian experience.

Most of the previously underdeveloped land downtown is either developed or, if it is still vacant, well maintained. An improved stock of buildings downtown has encouraged business owners to invest in their properties. The old system of angle parking has been removed. Ample public and private parking, including well located accessible parking for those who need it, is available for the benefit of downtown businesses and their patrons. Parking structures are developed underground, with green roofs and power generation using wind

energy and solar panels. The Town has invested in downtown beautification with tree planting, widened sidewalks, boulevards, flower baskets, landscaping, outdoor tables and chairs, street lights, and an information centre. Local attractions include large public murals and a museum that celebrates our agricultural and railway heritage.

Crossfield's walkable downtown is well linked to the rest of town with pathways and bicycle trails. In the long term, a rail station will be developed downtown to serve commuters and the general public taking advantage of the regional rapid transit system.

This Vibrant Main Street Vision is encapsulated in the Goal for Crossfield's Downtown and Entrance Area Redevelopment Plan:

Goal

We value our lively and vibrant main street, which contains unique, beautiful and artistic shops, retail stores, and service business – with a prosperous feel that reflects our pride in downtown Crossfield.

2.0 Interpretation and Background

2.1 Map Interpretation

Unless otherwise specified within the ARP, the boundaries or locations of any symbols or areas shown on these ARP maps are approximate only and shall be interpreted as such. They are not intended to define exact locations except where they coincide with clearly recognisable physical features or fixed boundaries, such as property lines or roads and utility rights-of-way.

2.2 Policy Interpretation

Where a descriptive section accompanies a policy, it is provided for information purposes only to enhance the understanding of the policy. Should an inconsistency of interpretation arise between the purpose section and a policy, the policy shall take precedence.

Where “**shall**” is used in a policy, the policy is considered mandatory. Nevertheless, where quantities or numerical standards are contained within mandatory policies, such quantities or standards may be varied so long as the intent of the policy is still achieved and the variation is necessary to address unique circumstances that would otherwise render compliance impractical or impossible.

Where “**should**” is used in a policy, the intent is that the policy is strongly encouraged, but can be varied where unique or unforeseen circumstances provide for courses of action that would satisfy the general intent of the policy.

Where a policy requires submission of studies, analysis, or information, the exact requirements and timing of the studies, analysis, or information shall be determined by the approving authority at the appropriate planning stage in accordance with Part 3 of this ARP, Implementation and Public Improvements.

2.3 Related Plans

All statutory planning documents, including Area Redevelopment Plans, must be consistent with each other. The primary statutory planning documents with which the Downtown and Entrance Area Redevelopment Plan must be consistent are the Town of Crossfield Municipal Development Plan and the Land Use Bylaw.

The Town of Crossfield Land Use Bylaw is an important statutory planning tool for implementing the ARP and the other statutory plans and municipal policies on a detailed and site-specific basis.

In addition to the approved statutory plans and the Land Use Bylaw, the Town of Crossfield Council has adopted by resolution various policy documents that set forth broad strategic goals or technical standards and guidelines for development. One important non-statutory plan that has guided the formulation of this ARP is the Crossfield Sustainability Plan. That plan, which Council approved in 2009, expresses both the vision of the community and key actions that need to be taken to implement the vision. The Crossfield Sustainability Plan and other non-statutory plans and policies of Council should be referred to on a regular basis in order to supplement the interpretation of this ARP. The Alberta Municipal Government Act, its regulations, and other provincial federal legislation should also be consulted when making decisions in accordance with this ARP.

2.4 Technical Background

Several of the engineering, transportation, and other technical studies supporting this ARP were prepared for the Town of Crossfield 2009 annexation application, which the Government of Alberta approved in June 2010. That annexation was approved to accommodate approximately 35 years of residential, commercial, and industrial growth to a total residential population of 12,000 to 15,000.

The future transportation network shown on Figure 8 - Transportation, the future stormwater management system shown on Figure 9 - Drainage Concept, the future water servicing concept shown on Figure 10 - Water Servicing, and the future sanitary sewer servicing concept shown on Figure 11- Wastewater Servicing, are all based upon transportation and engineering studies prepared for the Town of Crossfield 2009 annexation application. The major wetlands and natural systems shown in Figure 2- Natural Areas are based upon a biophysical overview that was specifically prepared in 2010 to support the MDP.

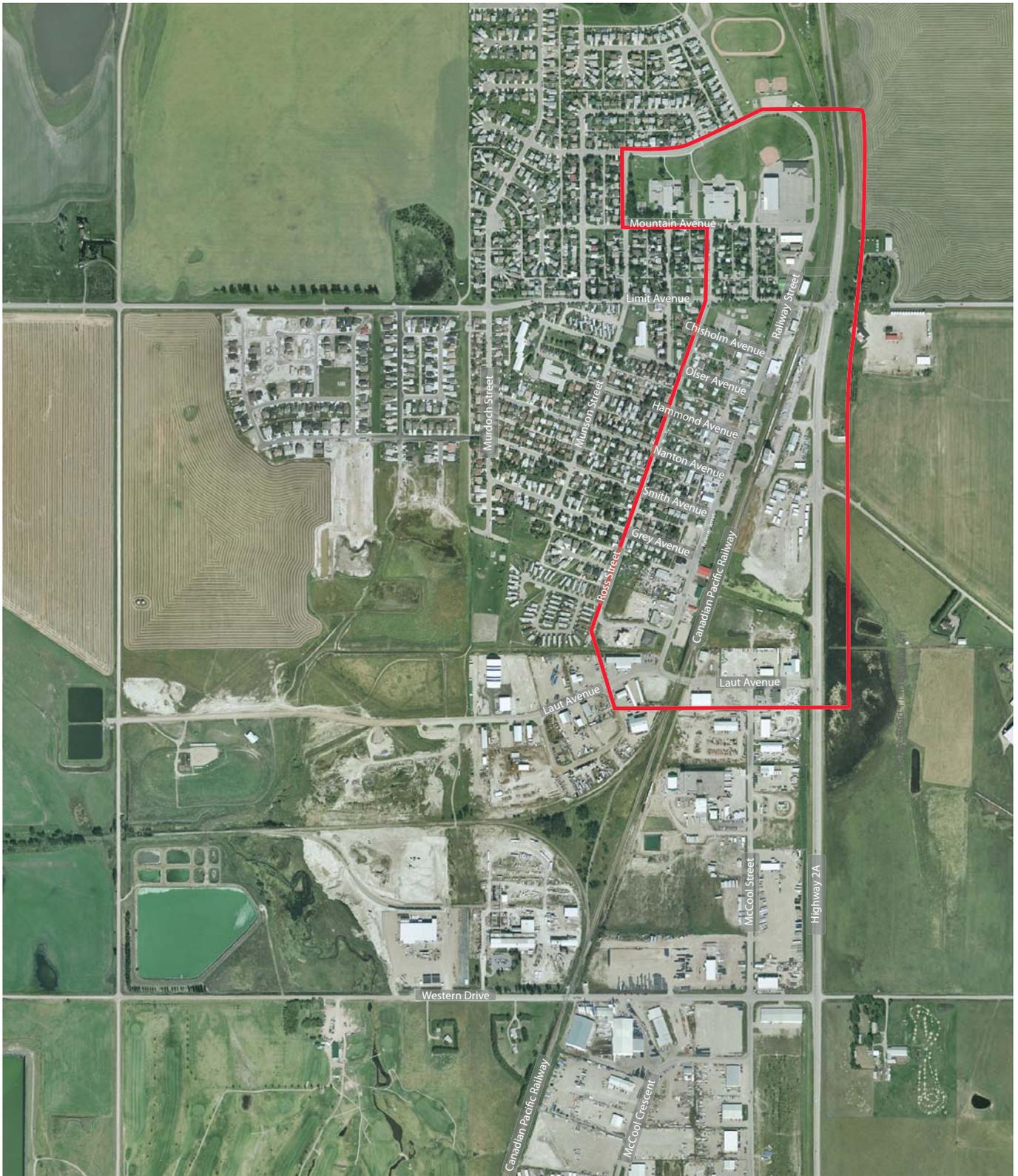
2.5 ARP Steering Committee

In March 2010 the Town of Crossfield Council adopted resolutions to authorise the preparation of this ARP and to establish an ARP Steering Committee to assist in the formulation of ARP policies. The ARP Steering Committee included local business people and landowners within the Downtown and Entrance Area, as well as municipal administration and members of the Town of Crossfield Council.

Steering Committee members assisted in two public open houses, one in May 2010 to explain the purpose of the ARP and one in September 2010 to present a draft of the ARP for public review and comment. In addition to overseeing the public consultation process for the ARP, the Steering Committee played a key role in:

- confirming a vision for downtown based upon the Crossfield Sustainability Plan
- providing advice in identifying issues and opportunities with respect to the ARP
- validating information and data to be used by municipal staff and the project team in the formulation of the ARP
- assisting municipal staff and the project team in facilitating stakeholder engagement during preparation of the ARP
- providing policy directions, solutions, and ideas to address issues and opportunities identified during the ARP process
- reviewing work of municipal staff and the project team to enhance accuracy and completeness
- recommending to Council the approval of the ARP It is also expected that the ARP Steering Committee will support the implementation of the ARP after Council approval

Figure 1
Downtown and Entrance Area Redevelopment Plan Boundary



 Downtown and Entrance ARP Boundary

3.0 Environmental Stewardship and Sustainability

3.1 Introduction

The physical environment and natural systems within the Downtown and Entrance Area, as identified in Figure 2 - Natural Areas, serve as a cornerstone for planning and development within this core area of Crossfield. Protection and enhancement of natural systems are among the key values of this ARP. These policies address both the environmental values of Crossfield and the means that the Town of Crossfield intends to use to achieve those goals.

3.2 Objectives

- 3.2.1 To protect and enhance all natural systems in an integrated and comprehensive manner.
- 3.2.2 To plan, develop, and build neighbourhoods, business and industrial districts, parks and open space systems, facilities, and community services based upon the principle that environmental protection and enhancement are the core values of Crossfield.
- 3.2.3 To involve all sectors of residential, business, industrial, and institutional communities in the protection and enhancement of natural systems within the Downtown and Entrance Area.

3.3 Policies

- 3.3.1 The Town of Crossfield shall protect the natural areas and systems within the Downtown and Entrance Area identified as semi-native grassland, woodland, tall shrub, and wetland shown in Figure 2- Natural Areas.
- 3.3.2 Future municipal and regional open space and pathway systems shall be based upon the natural areas and systems shown in Figure 2 - Natural Areas.

3.3.3 The Town of Crossfield shall work with local community groups, the Nose Creek Watershed Partnership, and both provincial and federal jurisdictions to ensure appropriate protection and management of the natural environment, including public open spaces, trails, and natural areas within and surrounding the Downtown and Entrance Area.

3.3.4 The Town of Crossfield shall collaborate with other levels of government and non-governmental, charitable, and community service organisations to facilitate the acquisition of privately owned land (or the rights of public access to such privately owned land) for the purposes of creating and protecting public open spaces, trails, and natural areas within and surrounding the Downtown and Entrance Area.

3.3.5 All proponents of development proposals adjacent to significant water bodies, such as the wetlands in the Downtown and Entrance Area, shall be responsible at the subdivision stage for delineating the 1:100 year floodplain, based upon the approval of a qualified engineer, to the satisfaction of the municipal approving authority.

3.3.6 The subdivision of lands within the 1:100 year floodplain, as identified by a qualified engineer, to the satisfaction of the municipal approving authority, shall not be permitted unless adequate flood-proofing measures are undertaken.

3.3.7 All proponents of development proposals adjacent to significant natural features, such as the wetlands in the Downtown and Entrance Area, shall be responsible at the subdivision stage for delineating the top of bank, based upon the approval of a qualified engineer, to the satisfaction of the municipal approving authority.

Figure 2
Natural Areas

-  Downtown and Entrance
ARP Boundary
-  Woodland
-  Tall Shrub
-  Wetland
-  Cultivated Wetland
-  Urban Anthropogenic
-  Rural Anthropogenic



North



Scale: NTS

- 3.3.8 Networks, locations, and alignments of trails within and surrounding the Downtown and Entrance Area shall be determined at the land use redesignation, subdivision, and development permit stages.
- 3.3.9 The planning and development of trails and parks within the Downtown and Entrance Area shall be coordinated among the Town of Crossfield, community groups, and business organisations such as the Crossfield Chamber of Commerce.
- 3.3.10 Landscaped elements of parks and open space systems may include buffers, berms, tree planting, or boulevards along major roadways.
- 3.3.11 At the land use redesignation, subdivision, or development permit stages, environmental impact assessments addressing natural areas on site or environmental site assessments addressing contamination on site shall be prepared by qualified environmental consultants, to the satisfaction of the municipal approving authority.
- 3.3.12 Lands identified as sensitive natural areas may be designated as Environmental Reserve at the subdivision stage in accordance with the Municipal Government Act. If at the subdivision stage private ownership of environmentally sensitive lands is deemed more appropriate by the municipal approving authority, then environmental reserve easements may be registered to protect the sensitive lands.
- 3.3.13 Owners of lands with existing tree stands will be encouraged to retain them to the extent possible at the time of development.
- 3.3.14 Where existing trees or tree stands cannot be retained as a result of development, the Town of Crossfield shall require the replacement of all trees that are removed because of such development.
- 3.3.15 Proponents of development proposals containing, adjacent to, or in the vicinity of a wetland, drainage course, land that is subject to flooding, land that abuts the bed and shore of a body of water, or land that in the opinion of the municipal approving authority is unstable, may be required to submit geotechnical or other studies that the hazards of developing in such areas.
- 3.3.16 The Town of Crossfield shall maintain information, as supplied by the Energy Resources Conservation Board (ERCB), indicating where oil and gas facilities are located within the Downtown and Entrance Area.
- 3.3.17 When considering land use redesignation, subdivision, and development permit proposals within the Downtown and Entrance Area, the Town of Crossfield shall apply ERCB setback regulations and guidelines respecting oil and gas facilities.
- 3.3.18 The Town of Crossfield shall cooperate to take advantage of funding opportunities and to exercise the legislative authority granted to municipalities under the MGA, the Historical Resources Act, and other provincial and federal legislation and programs, to conserve and protect historic buildings and sites as well as prehistoric and palaeontological features of significant heritage value to the Crossfield area.
- 3.3.19 The Town of Crossfield shall collaborate with government agencies, community groups, recreation groups, boards, and foundations to rehabilitate and maintain existing trails, parks, and natural areas within the Downtown and Entrance Area.
- 3.3.20 The Town of Crossfield shall collaborate with government agencies, community groups, environmental groups, boards, and foundations to provide recycling and composting bins, receptacles, and depots within the Downtown and Entrance Area.

4.0 Economic Development

4.1 Introduction

A number of factors indicate that Crossfield is well positioned for economic growth and development. These include, but are not limited to, the following:

- strategic location along the Canadian Pacific main line
- situation near the QE II Highway
- proximity to Calgary International Airport
- established and growing industrial development in Crossfield and surrounding area

A key component of Crossfield's economic development strategy will be the revitalisation and redevelopment of the Downtown and Entrance Area. The economic development policies of this ARP aim to link future revitalisation and redevelopment within the Downtown and Entrance Area with public improvements and the attraction of new business and development to Crossfield.

4.2 Objectives

- 4.2.1 To achieve sustained and balanced economic development that will provide a broad range of employment opportunities.
- 4.2.2 To build and take advantage of economic partnerships with local and regional businesses and agencies.

4.3 Policies

- 4.3.1 The Town of Crossfield shall dedicate staff and resources to pursue regional and intermunicipal economic development opportunities, to market existing businesses in Crossfield, and to attract new commercial and industrial development to the Downtown and Entrance Area.
- 4.3.2 The Town of Crossfield shall actively pursue opportunities to support and work with governmental and non-governmental organisations that may include the Governments of Alberta and Canada, other economic and tourism development agencies, the Canadian Pacific Railway, and other utility companies and service providers, in order to benefit economic development within the Downtown and Entrance Area.
- 4.3.3 The Town of Crossfield shall balance market imperatives that create economic development opportunities through competition with the need to sustain local businesses within the Downtown and Entrance Area, primarily by:
- providing incentives for development within the Downtown and Entrance Area, particularly along Railway Street
 - requiring standards for new development that both respect and enhance the local business environment
 - implementing public improvements, particularly along Railway Street, that will attract private investment in the Downtown and Entrance Area

- 4.3.4 The economic development strategy for the Town of Crossfield shall include the encouragement, facilitation, and approval of businesses within the Downtown and Entrance Area that contribute to the vitality of Crossfield by maintaining business and operating hours during evenings and weekends.
- 4.3.5 The Town of Crossfield shall share resources and information with local and regional businesses and agencies to support economic development within the Downtown and Entrance Area.
- 4.3.6 The Town of Crossfield shall facilitate the exchange of information, goods, and services among businesses within the Downtown and Entrance Area.
- 4.3.7 The economic development strategy for the Downtown and Entrance Area shall place the highest priority on the attraction of businesses and industries related to agriculture, green building, environmental protection, sustainable resource development, and energy efficiency.
- 4.3.8 The Town of Crossfield shall support and promote tourism activities, facilities, and businesses that will benefit the Downtown and Entrance Area.
- 4.3.9 A key component of the economic development strategy for the Town of Crossfield shall be to ensure that sound planning, development controls, and architectural guidelines enhance economic development within the Downtown and Entrance Area.

5.0 Land Use and Development

5.1 Introduction

The primary land use and development goal of this ARP is to engage in sound and efficient planning for streetscape enhancements and other public improvements, infrastructure, community facilities, and redevelopment of existing built areas, in order to improve the quality of life, to protect the natural environment, and to provide for economic vitality in Crossfield.

The land use and development objectives and policies of this ARP are intended to ensure an efficient and sustainable balance of residential, commercial, light industrial, institutional, community, and open space uses within the Downtown and Entrance Area.

These land use and development policies shall apply to the respective policy areas shown on Figure 3 - Land Use Policy Areas.

5.2 Objectives

- 5.2.1 To sustain the balanced development of an appropriate mix of land uses that will provide a variety of housing, employment, educational, and recreational opportunities within the Downtown and Entrance Area.
- 5.2.2 To efficiently develop high-quality multi-unit residential, commercial, institutional, light industrial, community, and open space land uses within the Downtown and Entrance Area for the benefit of all residents and businesses in Crossfield.

5.3 General Land Use and Development Policies

- 5.3.1 All new development and redevelopment within the Downtown and Entrance Redevelopment Area shall:
 - achieve a quality that meets or exceeds the highest development standards of the Town of Crossfield
 - be subject to development standards, landscaping requirements, and design elements that will give residents, local business people, and visitors positive visual impressions of Crossfield
- 5.3.2 All new development and redevelopment within the Downtown and Entrance Redevelopment Area shall be comprehensively designed to maximise efficiencies, to create attractive business, retail, and residential environments, and to provide support services for the benefit of customers, employees, tourists, and area residents.
- 5.3.3 All new development and redevelopment within the Downtown and Entrance Redevelopment Area should incorporate design that:
 - reduces the dependence of customers, employees tourists, and area residents on the private automobile for access to and from the commercial area
 - includes a variety of building types
 - exceeds minimum standards for site planning, landscaping, and building materials
 - supports pedestrian and other non-motorised transportation modes
 - provides access to open space and recreational areas and facilities

- 5.3.4 All new development and redevelopment within the Downtown and Entrance Redevelopment Area should incorporate attributes of sustainable development such as:
- alternative energy sources
 - energy efficiency
 - water conservation
 - recycled construction materials
 - green roofs and green walls
 - LEED, Green Build, or equivalent standards
 - Universal Design
 - Crime Prevention Through Environmental Design (CPTED)

5.3.5 All development and redevelopment within the Downtown and Entrance Redevelopment Area shall be required to connect to municipal infrastructure and services.

5.4 Railway Street Redevelopment Area

5.4.1 In conjunction with the economic development policies of this ARP, the Town of Crossfield shall promote an appropriate mix of high-quality multi-unit residential, commercial, institutional, light industrial, community, and open space land uses within the Railway Street Redevelopment Area.

- 5.4.2 The maximum building height for all development and redevelopment within the Railway Street Redevelopment Area shall be:
- 20 metres for all structures within 12 metres of the front property line adjacent to Railway Street
 - 33 metres for all structures further than 12 metres from the front property line adjacent to Railway Street.

5.4.3 Individual developments within the Railway Street Redevelopment Area shall be encouraged to include a mix of multi-unit residential, commercial, institutional,

community, and open space land uses, which shall be integrated at the appropriate scale, location, and proportions necessary to create a complete and vibrant urban community.

5.4.4 In conjunction with the economic development policies of this ARP, the Town of Crossfield shall encourage, facilitate, and approve businesses within the Railway Street Redevelopment Area that contribute to the vitality of Crossfield by maintaining business and operating hours during evenings and weekends.

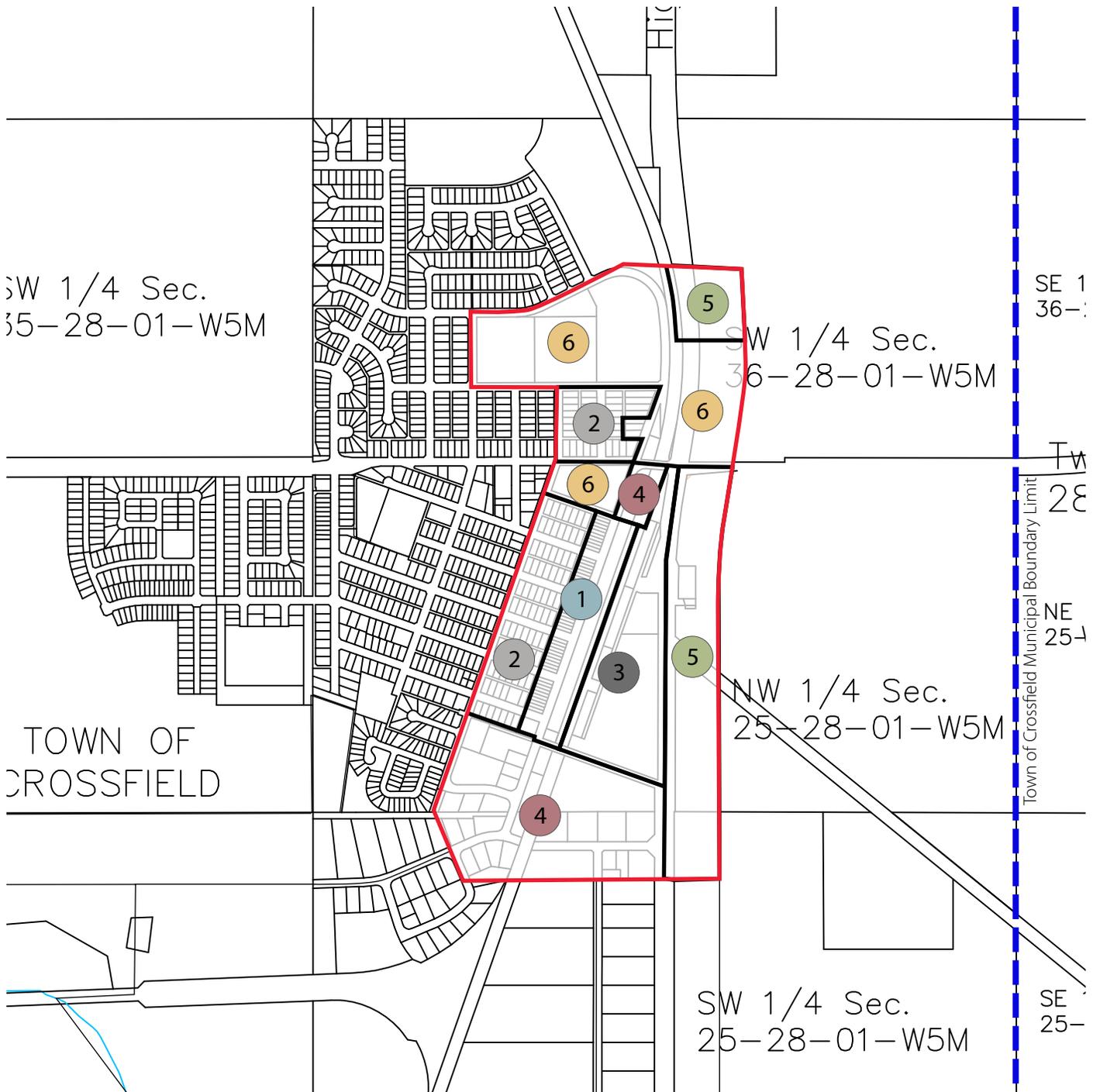
5.4.5 Large areas of surface parking within the Railway Street Redevelopment Area are discouraged, particularly where parking can be provided underground or behind buildings.

- 5.4.6 Street parking along Railway Street shall be subject to time limits and shall include:
- parallel parking along the entire length of the east side of Railway Street
 - parallel parking on the north and south ends of the west side of Railway Street
 - angle parking at a 30° angle to the curb along the west side of Railway Street between Osler Avenue and Grey Avenue, which the Town of Crossfield shall gradually convert to parallel parking

5.4.7 The Town of Crossfield shall work with private landowners and business operators to develop off-street parking for public use within the Railway Street Redevelopment Area, including an off-street parking lot on the east side of Railway Street.

5.4.8 Long-term parking of large transport trucks and trailers shall be prohibited and parking time limits for all other types of vehicles shall be required within the Railway Street Redevelopment Area.

Figure 3
Land Use Policy Areas



- | | | | |
|---|---------------------------------------|---|--|
|  | Downtown and Entrance ARP Boundary |  | Elevator Road Redevelopment Area |
|  | Town of Crossfield Municipal Boundary |  | Gateway and Entrance Redevelopment Areas |
|  | Railway Street Redevelopment Area |  | Greenfield Commercial Development Area |
|  | West Downtown Redevelopment Area |  | Municipal and Institutional Redevelopment Area |

- 5.4.9 In accordance with the public improvement program set forth in section 10.0 of this ARP, the Town of Crossfield shall raise funds to provide for public improvements within the Railway Street Redevelopment Area through measures such as redevelopment levies, local improvement bylaws, tax increment financing, or dedicated business taxes.
- 5.4.10 Sidewalks shall be provided on both sides of all streets within the Railway Street Redevelopment Area in accordance with the public improvement program set forth in section 10.0 of this ARP.
- 5.4.11 Multi-unit residential development shall be encouraged within the Railway Street Redevelopment Area.
- 5.4.12 All multi-unit residential development within the Railway Street Redevelopment Area shall achieve:
- minimum densities of 40 dwelling units per hectare (approximately 16 dwelling units per acre)
 - maximum densities of 80 dwelling units per hectare (approximately 32 dwelling units per acre)
- 5.4.13 The Town Crossfield shall encourage proponents of residential development within the Railway Street Redevelopment Area to achieve densities higher than the minimum residential densities established in policy 5.4.12.
- 5.4.14 The minimum residential densities required and encouraged within the Railway Street Redevelopment Area should be achieved through a variety of multi-unit housing types including townhouses, apartment dwellings, and live-work units.
- 5.4.15 Where multi-unit residential development is integrated with other non-residential uses within the Railway Street Redevelopment Area,
- the residential portions of the development shall:
- provide access to the residential components separate from the access to non-residential uses
 - where appropriate, provide for separate access at grade to individual dwelling units such as townhouses
 - include high-quality noise attenuation
 - provide for private and communal amenity areas that are separate from the non-residential uses
 - include a variety of dwelling unit types and sizes
 - accommodate pedestrians, cyclists, and other residents not necessarily dependent upon the automobile for their daily travelling needs
 - incorporate principles of Universal Design in site planning and architecture
- 5.4.16 New commercial development or redevelopment within the Railway Street Redevelopment Area shall:
- be of a scale that is appropriate in the context of the existing commercial uses within the Railway Street Redevelopment Area
 - include retail, personal service, arts and crafts, day care, medical, and professional office businesses that serve the local population
 - include hotels, restaurants, and cafés that add vitality to the Railway Street Redevelopment Area
 - include uses that do not require or generate large volumes of automobile traffic or large amounts of parking
 - accommodate pedestrians, cyclists, and other patrons not necessarily dependent upon the automobile for travelling to the Railway Street Redevelopment Area

5.4.17 Significant regional commercial development that requires or generates large volumes of automobile traffic or large amounts of parking shall not be developed within the Railway Street Redevelopment Area.

5.4.18 Institutional development and redevelopment shall be encouraged within the Railway Street Redevelopment Area and shall include institutional uses such as:

- municipal and other governmental offices, agencies, service providers, and meeting spaces
- meeting and office spaces for civic, business, and community organisations
- public and private schools, institutions of higher learning, and providers of vocational, adult, and distance education
- day cares and pre-schools
- public health care facilities

5.4.19 Community facilities and open spaces shall be encouraged within the Railway Street Redevelopment Area and shall include community and open space uses such as:

- public parks, playgrounds, and natural areas
- pathways, bicycle paths, and trails
- street furniture, public plazas, and urban amenity areas
- public libraries
- public markets
- community halls and meeting spaces for community organisations, clubs, and service organisations
- arts and culture facilities
- recreational facilities

5.4.20 Light industrial uses may only be developed or redeveloped within the Railway Street

Redevelopment Area if those uses will have no impacts on neighbouring lands and if they achieve a quality of development that meets or exceeds the highest commercial development standards of the Town of Crossfield.

5.5 West Downtown Redevelopment Area

5.5.1 Land uses within the West Downtown Redevelopment Area shall include an appropriate mix of commercial, institutional, community, open space, and all types of residential development.

5.5.2 The maximum building height for all development and redevelopment within the West Downtown Redevelopment Area shall be 15 metres.

5.5.3 Individual developments within the West Downtown Redevelopment Area shall be encouraged to include a mix of commercial, institutional, community, open space, and all types of residential development, which shall be integrated at the appropriate scale, location, and proportions necessary to create a complete and vibrant urban community.

5.5.4 Innovative site planning and architecture shall be strongly encouraged within the West Downtown Redevelopment Area. In order to achieve such innovation, the Town of Crossfield shall consider implementing incentives and bonus systems in the bylaws, regulations, and approval processes that apply to downtown Crossfield.

5.5.5 Large areas of surface parking within the West Downtown Redevelopment Area are discouraged, particularly where parking can be provided underground or behind buildings.

- 5.5.6 All street parking along and Ross Street and the Avenues within the West Downtown Redevelopment Area shall be parallel parking.
- 5.5.7 The Town of Crossfield shall work with private landowners and business operators to develop off-street parking for public use within the West Downtown Redevelopment Area.
- 5.5.8 Long-term parking of large transport trucks and trailers shall be prohibited and parking time limits for all other types of vehicles shall be required within the West Downtown Redevelopment Area.
- 5.5.9 In accordance with the public improvement program set forth in section 10.0 of this ARP, the Town of Crossfield shall raise funds to provide for public improvements within the West Downtown Redevelopment Area through measures such as redevelopment levies, local improvement bylaws, tax increment financing, or dedicated business taxes.
- 5.5.10 Sidewalks shall be provided on both sides of all streets within the West Downtown Redevelopment Area in accordance with the public improvement program set forth in section 10.0 of this ARP.
- 5.5.11 All types of residential development, including townhouses, apartment dwellings, and live-work units, shall be encouraged within the West Downtown Redevelopment Area.
- 5.5.12 All residential development and redevelopment within the West Downtown Redevelopment Area shall have a maximum density of 60 dwelling units per hectare (approximately 24 dwelling units per acre).
- 5.5.13 Residential redevelopment within the West Downtown Redevelopment Area shall result in a higher density than the residential development that is being replacing.
- 5.5.14 Where residential development is integrated with other non-residential uses within the West Downtown Redevelopment Area, the residential portions of the development shall:
- provide access to the residential components separate from the access to non-residential uses
 - where appropriate, provide for separate access at grade to individual dwelling units such as townhouses
 - include high-quality noise attenuation
 - provide for private and communal amenity areas that are separate from the non-residential uses
 - include a variety of dwelling unit types and sizes
 - accommodate pedestrians, cyclists, and other residents not necessarily dependent upon the automobile for their daily travelling needs
 - incorporate principles of Universal Design in site planning and architecture
- 5.5.15 New commercial development or redevelopment within the West Downtown Redevelopment Area shall:
- be of a scale that is appropriate in the context of the existing commercial uses within the West Downtown Redevelopment Area
 - include retail, personal service, arts and crafts, day care, medical, and professional office businesses that serve the local population
 - include restaurants and cafés that add vitality to the Railway Street Redevelopment Area
 - include uses that do not require or generate large volumes of automobile traffic or large amounts of parking

- accommodate pedestrians, cyclists, and other patrons not necessarily dependent upon the automobile for travelling to the West Downtown Redevelopment Area
- incorporate principles of Universal Design in site planning and architecture

5.5.16 Neither hotels nor significant regional commercial development that requires or generates large volumes of automobile traffic or large amounts of parking shall be developed within the West Downtown Redevelopment Area.

5.5.17 Institutional development and redevelopment shall be encouraged within the West Downtown Redevelopment Area and shall include institutional uses such as:

- municipal and other governmental offices, agencies, service providers, and meeting spaces
- meeting and office spaces for civic, business, and community organisations
- public and private schools, institutions of higher learning, and providers of vocational, adult, and distance education
- day cares and pre-schools
- public health care facilities

5.5.18 Community facilities and open spaces shall be encouraged within the West Downtown Redevelopment Area and shall include community and open space uses such as:

- public parks, playgrounds, and natural areas
- pathways, bicycle paths, and trails
- street furniture, public plazas, and urban amenity areas
- public libraries
- public markets
- community halls and meeting spaces for community organisations, clubs, and service organisations

- arts and culture facilities
- recreational facilities

5.5.19 Light industrial uses shall not be developed or redeveloped within the West Downtown Redevelopment Area.

5.6 Elevator Road Redevelopment Area

5.6.1 In conjunction with the economic development policies of this ARP, the Town of Crossfield shall promote an appropriate mix of high-quality commercial, light industrial, and open space land uses within the Elevator Road Redevelopment Area.

5.6.2 The maximum building height for all development and redevelopment within the Elevator Road Redevelopment Area shall be 48 metres.

5.6.3 Street parking within the Elevator Road Redevelopment Area shall not be permitted along Highway 2A.

5.6.4 The Town of Crossfield shall work with Alberta Transportation, Alberta Environment, the CPR, private landowners, and business operators within the Elevator Road Redevelopment Area to resolve the stormwater, sanitary sewer, water servicing, and highway access issues affecting redevelopment in that area.

5.6.5 The Town of Crossfield may raise funds to provide for public improvements and municipal infrastructure within the Elevator Road Redevelopment Area through measures such as redevelopment levies, local improvement bylaws, tax increment financing, or dedicated business taxes.

5.6.6 Large areas of surface parking within the Elevator Road Redevelopment Area are discouraged, particularly where parking can be provided underground or behind buildings.

- 5.6.7 New commercial development or redevelopment within the Elevator Road Redevelopment Area shall include retail and office uses that serve the regional trade area and the local population.
- 5.6.8 Regional commercial uses may be developed within the Elevator Road Redevelopment Area, provided that adequate parking as well as safe and efficient access to and from Highway 2A can be provided for the automobile traffic that such development would generate.
- 5.6.9 Institutional development within the Elevator Road Redevelopment Area shall include institutional uses such as:
- municipal and other governmental offices, agencies, service providers, and meeting spaces
 - meeting and office spaces for civic, business, and community organisations
 - institutions of higher learning and providers of vocational, adult, and distance education
- 5.6.10 Open spaces within the Elevator Road Redevelopment Area shall include regional parks and natural areas with pathways, bicycle paths, and trails that have safe and direct access to the rest of Crossfield.
- 5.6.11 Light industrial uses may be developed or redeveloped within the Elevator Road Redevelopment Area, provided they achieve a quality of development that meets or exceeds the highest commercial development standards of the Town of Crossfield.
- 5.6.12 Live-work uses may be developed within the Elevator Road Redevelopment Area, provided that the residential components of the live-work units:
- provide access to the residential components separate from the access to non-residential uses
 - include high-quality noise attenuation
 - provide for private and communal amenity areas that are separate from the non-residential uses
 - accommodate pedestrians, cyclists, and other residents not necessarily dependent upon the automobile for their daily travelling needs
 - incorporate principles of Universal Design in site planning and architecture
- 5.7 Gateway and Entrance Redevelopment Areas**
- 5.7.1 In conjunction with the economic development policies of this ARP, the Town of Crossfield shall promote an appropriate mix of high-quality commercial, institutional, light industrial, community, and open space land uses within the Gateway and Entrance Redevelopment Areas.
- 5.7.2 The maximum building height for all development and redevelopment within the Gateway and Entrance Redevelopment Areas shall be:
- 20 metres for all structures within 12 metres of the front property line
 - 33 metres for all structures further than 12 metres from the front property line
- 5.7.3 Large areas of surface parking within the Gateway and Entrance Redevelopment Areas are discouraged, particularly where parking can be provided underground or behind buildings from adjacent public roads.
- 5.7.4 Long-term parking of large transport trucks and trailers shall be prohibited and parking time limits for all other types of vehicles shall be required within the Gateway and Entrance Redevelopment Areas.

5.7.5 New commercial development or redevelopment within the Gateway and Entrance Redevelopment Areas shall include:

- retail and office uses that serve the regional trade area and the local population
- personal service, arts and crafts, and professional office businesses
- hotels, restaurants, and cafés that add vitality to the Railway Street Redevelopment Area
- principles of Universal Design in site planning and architecture

5.7.6 Regional commercial uses may be developed within the Gateway and Entrance Redevelopment Areas, provided that adequate parking as well as safe and efficient access to and from the adjacent public roads can be provided for the automobile traffic that such development would generate.

5.7.7 Institutional development within the Gateway and Entrance Redevelopment Areas shall include institutional uses such as:

- municipal and other governmental offices, agencies, service providers, and meeting spaces
- meeting and office spaces for civic, business, and community organisations
- institutions of higher learning and providers of vocational, adult, and distance education

5.7.8 Light industrial uses may only be developed or redeveloped within the Gateway and Entrance Redevelopment Areas if those uses will have no impacts on neighbouring lands and if they achieve a quality of development that meets or exceeds the highest commercial development standards of the Town of Crossfield.

5.7.9 Community facilities and open spaces shall be encouraged within the Gateway and Entrance

Redevelopment Areas and shall include community and open space uses such as:

- public parks and natural areas
- pathways, bicycle paths, and trails
- public libraries
- public markets
- community halls and meeting spaces for community organisations, clubs, and service organisations
- arts and culture facilities
- recreational facilities

5.7.10 The Town of Crossfield may raise funds to provide for public improvements, landscaping, gateway features, and municipal infrastructure within the Gateway and Entrance Redevelopment Areas through measures such as redevelopment levies, local improvement bylaws, tax increment financing, or dedicated business taxes.

5.8 Greenfield Commercial Development Area

5.8.1 Land use within the Greenfield Commercial Development Area shall include a mix of office, business, and retail development, with integrated supporting uses of the appropriate scale and location necessary to create a complete and vibrant urban community. Such supporting uses shall include adequate open space and may also include:

- commercial and retail development to serve immediate employees and business visitors
- regional commercial and retail development
- institutional development of a regional scale, including indoor and outdoor recreation facilities
- tourism destinations and services such as hotels

- schools and institutions of higher learning
 - personal service businesses, medical offices, health centres, and hospitals
- 5.8.2 For the purposes of this ARP, “regional commercial development” - significant office and retail commercial development outside of predominantly residential neighbourhoods - shall be developed within the Greenfield Commercial Development Area in order to serve and provide employment opportunities for the residents of Crossfield and the surrounding area.
- 5.8.3 New regional commercial development and subdivision within the Greenfield Commercial Development Area shall be based upon the prior approval of Area Structure Plans in accordance with Section 633 of the Alberta Municipal Government Act.
- 5.8.4 Area Structure Plans and subdivision approvals to accommodate significant regional commercial development should take into consideration:
- engineering studies
 - cost-benefit analyses
 - commercial and retail market studies
 - both capital investment in and maintenance of public infrastructure
- 5.8.5 Where new commercial development and subdivision within the Greenfield Commercial Development Area are proposed adjacent to or within view of existing or future commercial or residential land uses, such proposed commercial development and subdivision shall:
- achieve development standards that meet or exceed the development standards of those neighbouring commercial or residential uses
 - be subject to development standards, landscaping requirements, design elements, traffic calming and management systems, and operational procedures that will serve to integrate the proposed commercial development with the neighbouring commercial or residential uses
- 5.8.6 The maximum building height for all development and redevelopment within the Greenfield Commercial Development Area shall be 48 metres.
- 5.8.7 Large areas of surface parking within the Greenfield Commercial Development Area are discouraged, particularly where parking can be provided underground or behind buildings.
- 5.8.8 Long-term parking of large transport trucks and trailers shall be prohibited and parking time limits for all other types of vehicles shall be required within the Greenfield Commercial Development Area.
- 5.8.9 Sidewalks should be provided on both sides of all streets in commercial development areas. Where design, engineering, or physical constraints render development of sidewalks on both sides of a street impractical, cause unsafe conditions, or create disadvantages for pedestrians, the developer shall compensate for the lack of sidewalks by enhancing trails or bicycle pathways within the same commercial development area.
- 5.8.10 Lands within the Greenfield Commercial Development Area may accommodate commercial or light industrial land uses, but shall be developed or redeveloped to achieve a quality of development that meets or exceeds the highest commercial development standards of the Town of Crossfield.

5.9 Municipal and Institutional Areas

- 5.9.1 Land use within the Municipal and Institutional Areas shall include parks, playgrounds, schools, community facilities, recreational facilities, municipal buildings, and other public services, with integrated supporting uses of the appropriate scale and location necessary to create a complete and vibrant urban community.
- 5.9.2 The Town of Crossfield shall support existing and new agreements with outside agencies to provide parks, open space, educational, recreational, and cultural facilities and services in the Crossfield area.
- 5.9.3 Agreements on the provision of parks, open space, educational, recreational, and cultural facilities and services should take into consideration:
- needs assessments
 - engineering studies
 - cost-benefit analyses
 - both capital investment and maintenance of infrastructure
 - staffing requirements and operating budgets
- 5.9.4 Where appropriate, the Town of Crossfield may require developers to provide financial contributions toward the development of parks, open space, educational, recreational, and cultural facilities within the Municipal and Institutional Areas.
- 5.9.5 New community facilities within the Municipal and Institutional Areas shall be designed in consideration of the long-term needs of an evolving population and, where appropriate, shall provide for future expansion or adaptation in anticipation of changing community needs.
- 5.9.6 The Town of Crossfield shall demonstrate leadership in environmental design and sustainability by ensuring that all municipal facilities achieve measurable benefits in moving the Crossfield area toward greater environmental, economic, fiscal, and social sustainability.
- 5.9.7 Municipal facilities developed by the Town of Crossfield should incorporate attributes of sustainable development such as:
- alternative energy sources
 - energy efficiency
 - water conservation
 - recycled construction materials
 - green roofs and green walls
 - LEED, Green Build, or equivalent standards
 - Crime Prevention Through Environmental Design (CPTED)
 - Universal Design
 - multi-use facilities that serve all ages and segments of the community
 - adaptive re-use of existing public facilities
 - strategic location of key public facilities
 - co-location of complementary public facilities and services
 - public education about sustainable development
 - public involvement in design and development
- 5.9.8 The Town of Crossfield shall cooperate with all public, separate, and private school authorities and institutions of higher learning to ensure efficient and effective delivery of education and educational support services within the Municipal and Institutional Areas.

6.0 Architectural and Design Guidelines

6.1 Introduction

The architectural style of a town can be an important factor in defining a place, its history and its identity. Although some consistency in form is important to achieve a feeling of cohesiveness throughout Crossfield, the guidelines set out in this section allow for flexibility in order to prevent a monotonous built form and to provide architectural variety and interest within the various Redevelopment Areas.

The overarching purpose of these guidelines is to promote good design without being overly prescriptive, so that Crossfield will be able to attract businesses that continue to add vitality to the Downtown and Entrance Area.

The design guidelines and principles outlined in this section focus on the private land within Crossfield's downtown core. This section contains overall objectives for the entire ARP; however, given the distinctive characteristics of each of the Redevelopment Areas (outlined in Section 5, Land Use and Development), it is useful to establish specific objectives according to the issues relevant to each of the areas. Architectural and design guidelines are set forth for all Redevelopment Areas with the exception of the West Downtown and Municipal and Institutional Redevelopment Areas, since generic policies have been considered in Section 5.4 and 5.8 of the ARP, and detailed architectural guidelines are not deemed necessary for those two Redevelopment Areas.

6.2 Objectives

- 6.2.1 To create a sense of place and identity for the town of Crossfield.
- 6.2.2 To support well-designed high quality and exemplary development that produces attractive and high-quality people places,

thereby creating conditions that sustain a flourishing economic and community life.

- 6.2.3 To encourage flexibility and creativity.
- 6.2.4 To promote sustainable initiatives through new development.

6.3 Railway Street Redevelopment Area

6.4 Introduction

This is the heart of Crossfield, and the commercial centre - where much of the activity occurs. Although Railway Street is currently a successful main street, it is generally accepted that the appearance of the street is tired and dated. The guidelines in this section provide a framework for revitalisation and new development, which will enhance and improve the image of Crossfield.

6.5 Objectives

- 6.5.1 To encourage the provision of a range of local and regional commercial mixed uses that would serve the immediate community as well as the regional area.
- 6.5.2 To improve and enhance the "active" street frontage along Railway Street through occupied retail units.
- 6.5.2 To promote development of an appropriate scale to ensure there is a defined sense of enclosure, through development on both sides of Railway Street.
- 6.5.3 To provide a cohesive approach to new development and redevelopment along Railway Street, utilising architectural styles and details from the same "palette".

6.6 Guidelines

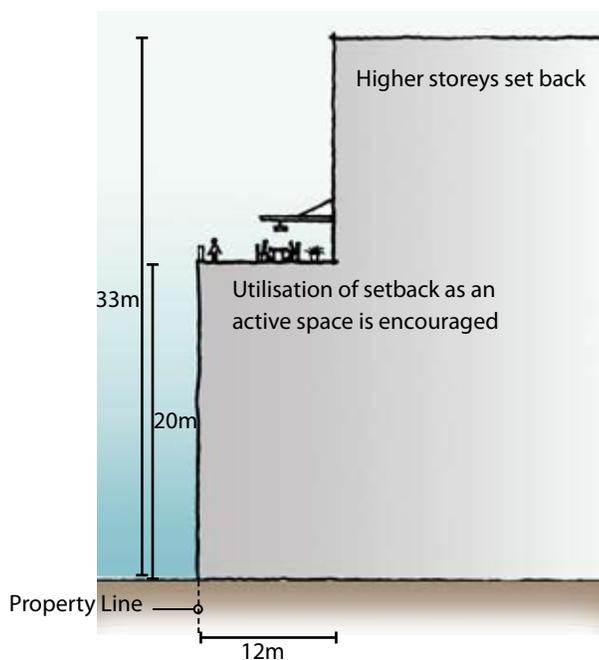
6.6.1 Scale and Massing

- The scale of new development should be in keeping with the surrounding context, with consideration of the maximum height policies set out in section 5.4.2, to ensure that Railway Street maintains a “human scale” of built form.

6.6.2 Built Form and Layout

- At least 75% of the development should build up to the property line (i.e. 0m setback); to ensure the defined street frontage is maintained.
- Setbacks should be permitted only if this space is utilised for pedestrian activity, such as arcades, building entries, cafes or other public space (refer to Figure 5).

Figure 4 Height Setbacks



- Large areas of surface parking are discouraged, particularly where parking can be provided underground or behind buildings.

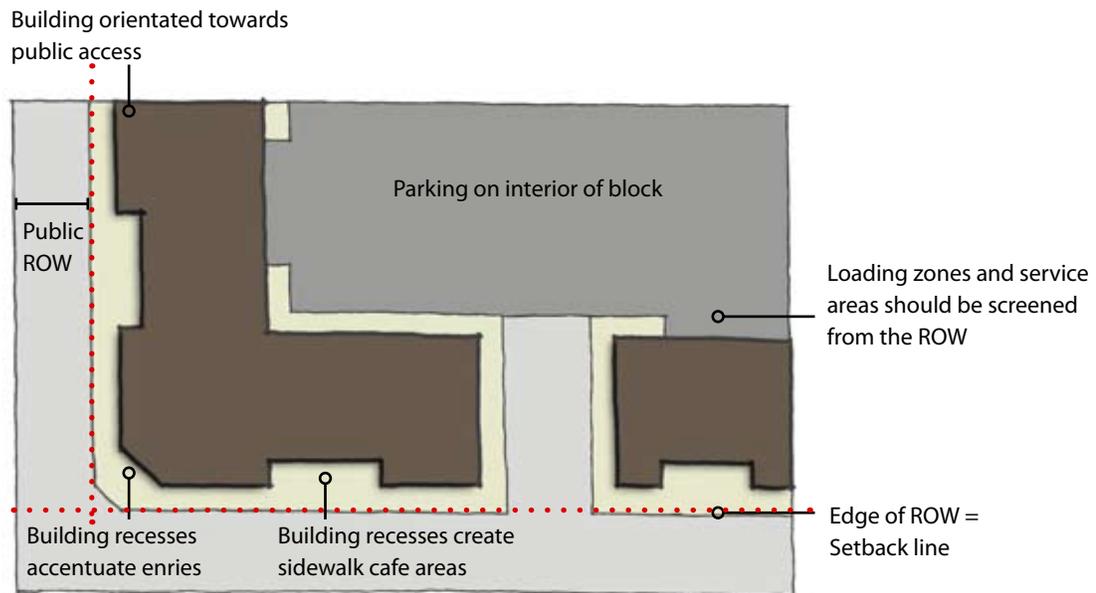
6.6.3 Screening

- Loading docks, commercial garbage storage, service courts and mechanical equipment should not be visible from public street rights-of-way. They should be located behind the building and screened from view with walls, fences or natural vegetation.

6.6.4 Elevations

- First floor heights should be between 4.2m (14ft) and 4.9 (16ft) high.
- Transparent windows and doors should cover at least 40% of building façades facing public streets to ensure indoor uses are visible.
- Windowless sections of wall should not exceed 6m (20ft) in length.
- Façades should present a pattern of architectural variety through detailing, color, texture and materials.
- Building façades should provide elements of architectural scale and proportion that relate to the storefront height, the overall building scale and the human scale of the pedestrian environment.
- Architectural elements should emphasise primary building entries, important corners or significant architectural features.

Figure 5 Building Line Setbacks



6.6.5 Roofs

- a. Building roof forms should respect the context in which they are viewed, in terms of height and proportions.
- b. Flat roofs should have a parapet wall or cornice detailing to provide animation to the roof line.
- c. All mechanical, electrical and communications systems should be screened from view from surrounding streets, highways and other buildings.

6.6.6 Canopies

- a. Generally, development permits should be required for the installation of store canopies.
- b. Where used, blinds and canopies should always be retractable so that the fascia is not permanently obscured.
- a. Folding canopies or blinds should have a minimum vertical clearance of 3m (9.8ft). A minimum horizontal clearance of 0.9m (3ft) shall be required from the edge of the right of way to the canopy.



Canopies add visual interest to the street

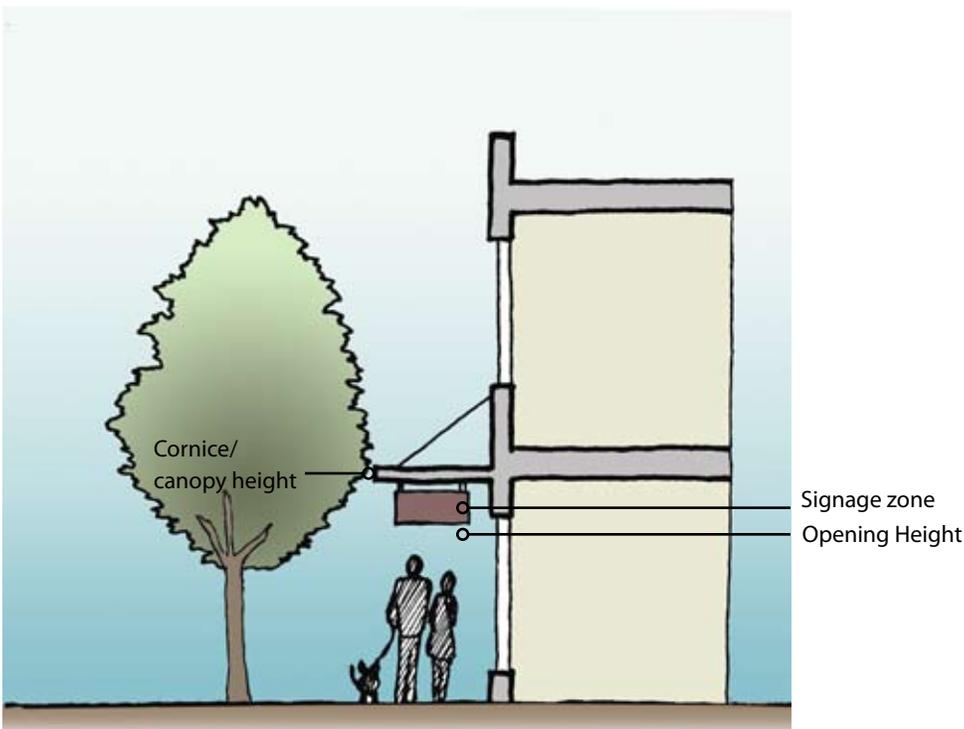
6.6.7 Signage

- a. Neon signs should not be used as the primary external signs for businesses along Railway Street.
- b. Freestanding temporary signs should not be permitted on Railway Street without the prior approval of a development permit.
- c. Retail signage should be positioned directly above the doorway opening within the 'signage zone' detailed in Figure 6.
- d. Projecting 'swing' signs will be encouraged with the approval of the Town of Crossfield to ensure the uniform appearance of the street.



Signs in similar styles provide a sense of cohesion on the street

Figure 6 Signage Zone



6.6.8 Materials

- a. Materials should be of the highest quality and be respectful to adjacent buildings and the immediate context.
- b. Large unbroken elevations comprised of a singular material should be avoided.
- c. The Town of Crossfield should support the use of locally sourced and recycled materials.
- d. The following materials should not be permitted: vinyl cladding; cinder blocks; and concrete.
- e. The following materials are considered acceptable: acrylic stucco (i.e. smooth or without texture), stone and slate.
- f. The following materials are encouraged: brick; wooden cladding; and energy efficient glazing.
- g. Materials should be selected from (but not restricted to) the following color palette:

Railway Street Preferred Colour Palette:



6.6.9 Landscaping

- a. Landowners should provide additional well maintained planting outside their property, provided it does not restrict pedestrian or cyclist movement along Railway Street.

6.6.10 Lighting

- a. Lighting to illuminate certain architectural features, such as entrances and towers is encouraged.
- b. Business owners along Railway Street are encouraged to provide lighting within store windows, for increased security after hours.
- c. Energy efficient and lighting powered by sustainable sources is strongly encouraged.

6.6.11 Façade Improvement Program

The town shall investigate the possibility of introducing a façade improvement incentive program for business owners along Railway Street. This could work in a number of ways:

1. Property owners or business tenants who install at least \$5,000 in improvements to the façade (s) of an existing building are eligible for a grant of up to 50% of the cost of the façade improvements (the “Improvement Cost”) upon their completion up to a maximum grant amount (to be determined by the Town of Crossfield).
2. The Town may offer a property tax credit based on the level of improvement on the store frontage.

6.7 Elevator Road Redevelopment Area

6.8 Introduction

This area occupies a central position in between Highway 2A and the CPR tracks. Its proximity to the Highway means that it plays an important role on the first impressions of Crossfield. The area has also been identified as an important natural habitat, through the presence of the existing wetlands. Although at the time of ARP approval this area accommodated light industrial and outside storage, these guidelines promote and encourage the redevelopment of this land.

6.9 Objectives

- 6.9.1 To provide high quality and exemplary development that sets the benchmark for Crossfield.
- 6.9.2 To encourage active use of the land with a strong visual presence to Highway 2A.

6.10 Guidelines

6.10.1 Scale and Massing

- a. Development should have a maximum building height of 48m and façades should have a minimum height of 10m — to help provide a sense of enclosure for Highway 2A.
- b. Development should be of an appropriate scale to the surrounding context and provide sufficient visual interest from Highway 2A.

6.10.2 Built Form and Layout

- a. Development should be oriented toward Highway 2A.
- b. Appropriate access points should be provided from Highway 2A, provided they do not have a detrimental impact on the movement of non-motorised traffic, pedestrians and environmental habitats.
- c. Parking should be located to the rear of the development, adjacent to the CPR railway, and should not dominate the area fronting the highway to ensure active frontage to Highway 2A and to utilise the noise setbacks from the railway for parking.

6.10.3 Screening

- a. Loading docks, commercial garbage storage, service courts and mechanical equipment should not be visible from public street rights-of-way. They should be located behind the building and screened from view with walls, fences or natural vegetation.

6.10.4 Elevations

- a. Façades should present a pattern of architectural variety through detailing, color, texture and materials.
- b. Building façades should provide elements of architectural scale and proportion that relate to the building height, the overall building scale and the human scale of the pedestrian environment.
- c. Architectural elements should emphasise primary building entries, important corners or significant architectural features.

6.10.5 Roofs

- a. Building roof forms should respect the context in which they are viewed, in terms of height and proportions.
- b. Parapets should conceal flat roofs.
- c. Variation in roofline is suggested to reduce the scale of large buildings.
- d. Overhanging eaves are encouraged to add detail on large buildings and to provide shelter for pedestrians.

- e. All mechanical, electrical and communications systems should be screened from view from surrounding streets, highways and other buildings.

6.10.6 Signage

- a. Neon signs should not be used as the primary external signs for businesses within the Elevator Road Redevelopment Area.
- b. Signs should be placed on the building frontage directly to promote the strong orientation towards the highway.
- c. Freestanding signs or billboards should not be permitted within the Elevator Road Redevelopment Area between development and Highway 2A.

6.10.7 Materials

- a. Materials should be of the highest quality and be respectful to adjacent buildings and the immediate context.
- b. Large unbroken elevations comprised of a singular material should be avoided.
- c. The Town of Crossfield should support the use of locally sourced and recycled materials.
- d. The following materials should not be permitted: vinyl cladding; cinder blocks; and concrete.
- e. Samples of materials may be requested by the Town of Crossfield prior to determining development permit applications.

6.10.8 Landscaping

- a. Appropriate natural screening should be provided along the CPR railway.
- b. Where stormwater and drainage ponds are required, these should be promoted as 'natural areas' that are fully accessible to the public.
- c. Appropriate landscaping should be provided in parking lots to reduce the visual impact of vehicles.

6.10.9 Lighting

- a. Lighting to illuminate certain architectural features, such as entrances and towers is encouraged.
- b. Business owners within the Elevator Road Redevelopment Area are encouraged to provide lighting within store windows, for increased security after hours.
- c. Parking lot lighting should be of a human scale and not have an adverse effect on users of Highway 2A.
- d. Energy efficient and lighting powered by sustainable sources is strongly encouraged.

6.11 Gateway and Entrance Redevelopment Areas

6.12 Introduction

These areas include the land focused around Laut Avenue to the south of Railway Avenue and Limit Avenue to the north. They are essential in terms of creating a strong positive impression of the town. Public improvements form part of the proposals for this Redevelopment Area, which is addressed in Section 10 of the ARP.

6.13 Objective

- 6.13.1 To provide landmark development that marks the entrance to the downtown core of Crossfield.

6.14 Guidelines

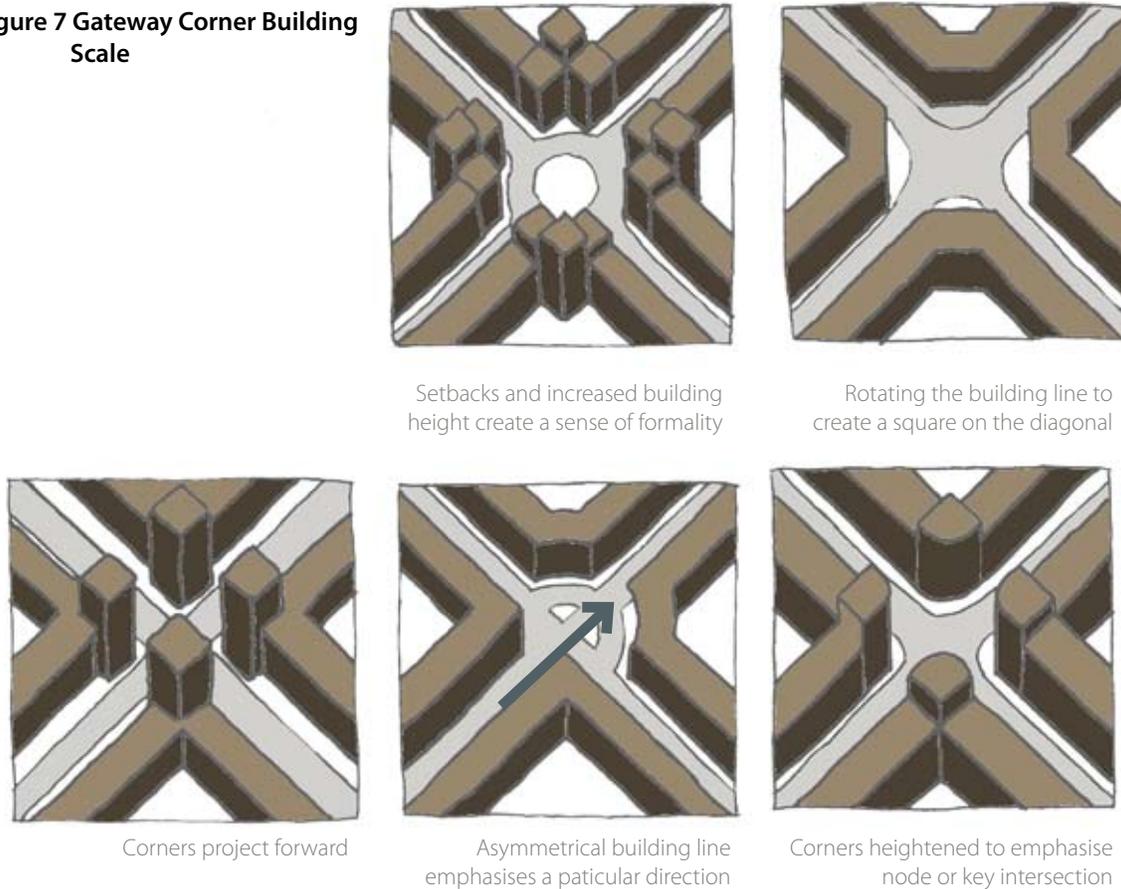
6.14.1 Scale and Massing

- a. Development should be of an appropriate scale to the surrounding context and provide sufficient visual interest for the entrance to the town.
- b. On corner sites, building height should increase at the intersection and decrease gradually, with towers of an appropriate scale being encouraged to animate key intersections.

6.14.2 Built Form and Layout

- a. Development should have a strong visual frontage to the street with minimal setbacks from the street.
- b. Only 'landmark' development that meets the highest design standards of the Town of Crossfield should be permitted on key intersections.
- c. Parking should be located to the rear of development.

Figure 7 Gateway Corner Building Scale



Setbacks and increased building height create a sense of formality

Rotating the building line to create a square on the diagonal

Corners project forward

Asymmetrical building line emphasises a particular direction

Corners heightened to emphasise node or key intersection

6.14.3 Screening

- a. Loading docks, commercial garbage storage, service courts and mechanical equipment should not be visible from public street right-of-way. They should be located behind the building and screened from view with walls, fences or natural vegetation.

6.14.4 Elevations

- a. On corner sites, active edges (including architectural details, windows etc.) should 'wrap' around the building to ensure there are no blank façades.
- b. Façades should present a pattern of architectural variety through detailing, color, texture and materials.
 - a. Building façades should provide elements of architectural scale and proportion that relate to the building height, the overall building scale and the human scale of the pedestrian environment.
 - b. Architectural elements should emphasise primary building entries, important corners or significant architectural features.

6.14.5 Roofs

- a. Building roof forms should respect the context in which they are viewed, in terms of height and proportions.
- b. Parapets should conceal flat roofs.
- c. Variation in roofline is suggested to reduce the scale of large buildings.
- d. Overhanging eaves are encouraged to add detail on large buildings and to provide shelter for pedestrians.
 - a. All mechanical, electrical and communications systems should be screened from view from surrounding streets, highways and other buildings.

6.14.6 Signage

- a. Neon signs should not be used as the primary external signs for businesses within the Gateway and Entrance Redevelopment Areas.
- b. Signs should be placed on the building frontage directly to promote the strong orientation towards the street.
- c. Freestanding signs or billboards should not be permitted to promote high standards of design and to reduce street clutter.



Careful design of corner buildings is extremely important on gateway sites

6.14.7 Materials

- a. Materials should be of the highest quality and be respectful to adjacent buildings and the immediate context.
- b. Large unbroken elevations comprised of a singular material should be avoided.
- c. The Town of Crossfield will support the use of locally sourced and recycled materials.
- d. The following materials should not be permitted: vinyl cladding; cinder blocks; and concrete.
- e. Samples of materials may be requested by the Town of Crossfield prior to determining development permit applications.

6.14.8 Landscaping

- a. On plot landscaping should be provided to enhance the streetscape.
- b. Appropriate landscaping should be provided in parking lots to reduce the visual impact of vehicles.

6.14.9 Lighting

- a. Lighting to illuminate certain architectural features, such as entrances and towers is encouraged.
- b. Business owners within the Gateway and Entrance Redevelopment Areas are encouraged to provide lighting within store windows, for increased security after hours.
- c. Parking lot lighting should be of a human scale and not have an adverse effect on nearby roadways or pedestrian areas.

- d. Energy efficient and lighting powered by sustainable sources is strongly encouraged.

6.14.10 Public Art

- a. The Town may impose a public art levy (as outlined in ARP policy 10.9.1) on new development to include an appropriate gateway feature at the intersections of Railway Street and Limit Avenue and Laut Avenue.

6.15 Greenfield Commercial Development Area

6.16 Introduction

The Greenfield Commercial Redevelopment Area provides the eastern frontage to Highway 2A, and sits opposite the Elevator Road Redevelopment Area. As such it provides an important frontage for those using Highway 2A. The prominent location means that standards of design should be exceptional, and expectations of this area go far beyond the standard commercial strip mall layout.

6.17 Objective

- 6.17.1 To promote exceptional design standards and encourage exemplar commercial development that the Town of Crossfield can be proud of.

6.18 Guidelines

6.18.1 Scale and Massing

- a. Development should have a maximum building height of 48m and a minimum height of 10m, to ensure an appropriate scale to the surrounding context and to help provide a sense of enclosure for Highway 2A.
- b. Development should be of an appropriate scale to the surrounding context and provide sufficient visual interest from Highway 2A.

6.18.2 Built Form and Layout

- a. Linear “strip” development is strongly discouraged. If this approach is used, the development should incorporate design elements that significantly mitigate the linear effect.
- b. Development should be oriented toward Highway 2A.
- c. Parking should be located to the rear of the development and not dominate the area fronting the highway, to ensure active frontage to Highway 2A.
- d. Big box retail should stack floors and parking to reduce its overall footprint and enhance the pedestrian experience.

6.18.3 Elevations

- a. On corner sites, active edges (including architectural details, windows etc.) should ‘wrap’ around the building to ensure there are no blank façades.
- b. Façades should present a pattern of architectural variety through detailing, color, texture and materials.

- c. Building façades should provide elements of architectural scale and proportion that relate to the building height, the overall building scale and the human scale of the pedestrian environment.
- d. Architectural elements should emphasise primary building entries, important corners or significant architectural features.

6.18.4 Screening

- a. Loading docks, commercial garbage storage, service courts and mechanical equipment should not be visible from public street right-of-way. They should be located behind the building and screened from view with walls, fences or natural vegetation.

6.18.5 Roofs

- a. Building roof forms should respect the context in which they are viewed, in terms of height and proportions.
- b. Parapets should conceal flat roofs.
- c. Variation in roofline is suggested to reduce the scale of large buildings.
- d. Overhanging eaves are encouraged to add detail on large buildings and to provide shelter for pedestrians.
- e. All mechanical, electrical and communications systems should be screened from view from surrounding streets, highways and other buildings.

6.18.6 Signage

- a. Neon signs should not be used as the primary external signs for businesses within the Greenfield Redevelopment Area.

- b. Signs should be placed on the building frontage directly to promote the strong orientation towards the street.
- c. Freestanding signs or billboards should not be permitted to promote high standards of design and to reduce roadway clutter.

6.18.7 Materials

- a. Materials should be of the highest quality and be respectful to adjacent buildings and the immediate context.
- b. Large unbroken elevations comprised of a singular material should be avoided.
- c. The Town of Crossfield will support the use of locally sourced and recycled materials.
- d. The following materials should not be permitted: vinyl cladding; cinder blocks; and concrete.
- e. Samples of materials may be requested by the Town of Crossfield prior to determining development permit applications.



Consistent frontlighting of signs provide unity on a main street

6.18.8 Landscaping

- a. On-site landscaping should be provided to enhance the streetscape.
- b. Appropriate landscaping should be provided in parking lots to reduce the visual impact of vehicles.

6.18.9 Lighting

- a. Lighting to illuminate certain architectural features, such as entrances and towers is encouraged.
- b. Business owners within the Greenfield Commercial Redevelopment Area are encouraged to provide lighting within store windows, for increased security after hours.
- c. Parking lot lighting should be of a human scale and not have an adverse effect on users of Highway 2A.
- d. Energy efficient and lighting powered by sustainable sources is strongly encouraged.



Example of backlit sign



Example of frontlit sign

7.0 Transportation

7.1 Introduction

Transportation systems will be critical in ensuring both economic development and quality of life within the Downtown and Entrance Area. The long-term transportation infrastructure planned for the Downtown and Entrance Area is identified in Figure 8 - Transportation. The Town of Crossfield is committed to the continual engagement of Alberta Transportation in all transportation matters. These transportation policies address both conventional roadway infrastructure and important alternatives to the private automobile such as public transit, cycling, and pedestrian modes of travel.

7.2 Objectives

- 7.2.1 To provide for a safe and efficient transportation system within the Downtown and Entrance Area for the benefit of all residents, businesses, employees, and visitors to Crossfield.
- 7.2.2 To sustain the balanced development of a complete range of transportation infrastructure within the Downtown and Entrance Area, which will provide a variety of automotive and non-automotive transportation choices for residents, businesses, employees, and visitors to Crossfield.

7.3 Policies

- 7.3.1 The Town of Crossfield shall coordinate the planning and development of a long-term transportation infrastructure system consistent with the network identified in Figure 8 - Transportation.

- 7.3.2 The Town of Crossfield shall collaborate with Alberta Transportation and the Canadian Pacific Railway (CPR) in the planning and development of the long-term transportation infrastructure system within the Downtown and Entrance Area, with particular reference to:

- traffic controls at CPR level crossings
- review of safety and speed concerns and standards within the Downtown and Entrance Area
- improvements to Highway 2A

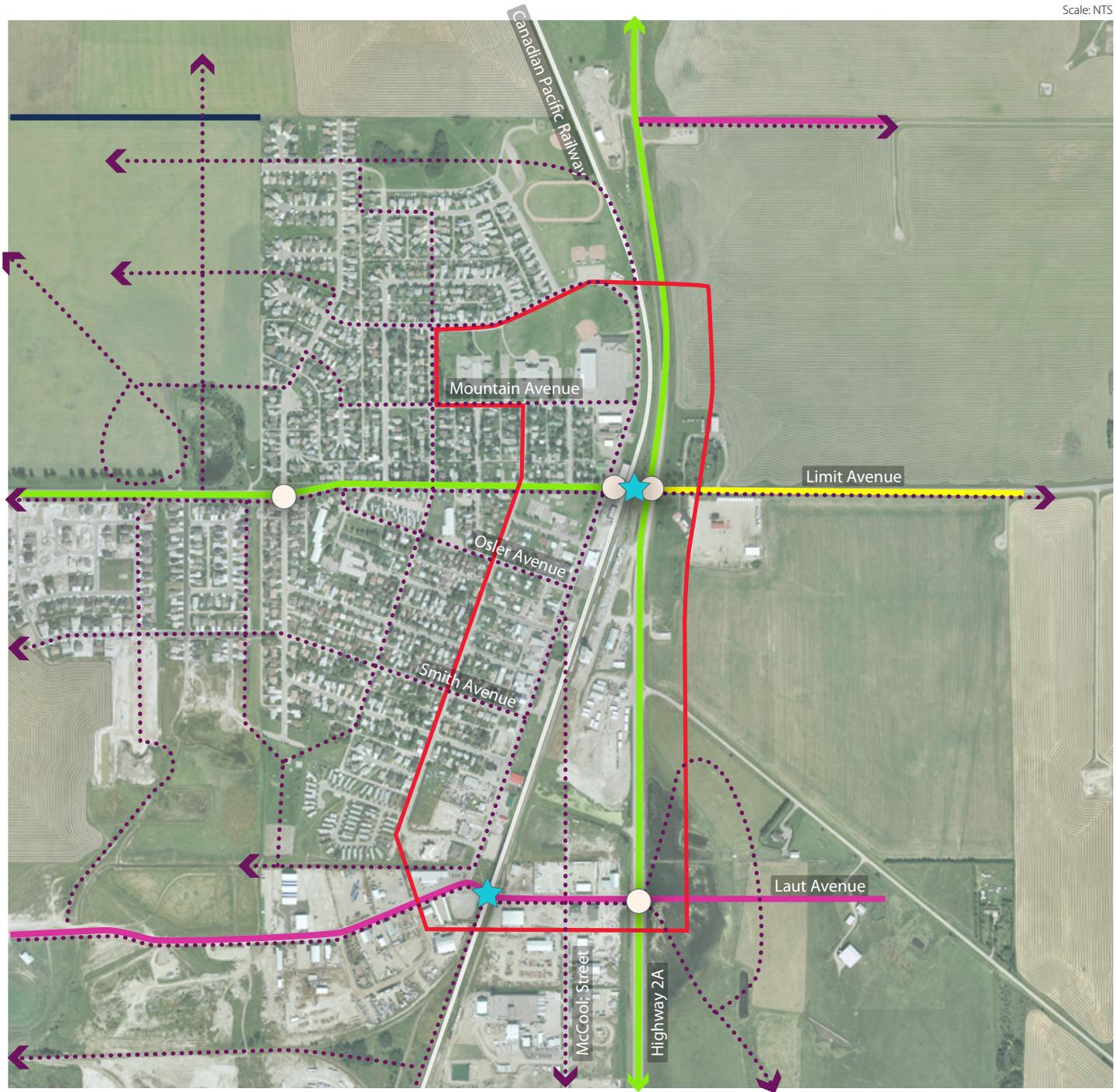
- 7.3.3 The Town of Crossfield shall support development, maintenance, and expansion of transportation networks within and around the Downtown and Entrance Area by clearly and consistently defining responsibilities and standards for roadway construction and maintenance, including the responsibilities of the municipality, Alberta Transportation, and developers.

- 7.3.4 The Town of Crossfield shall require that development and subdivision along major roadways within the Downtown and Entrance Area:

- obtain approvals, as required, from Alberta Transportation
- coordinate the number of entry and exit points to major roadways
- provide sufficiently wide rights-of-way or setbacks to accommodate berms, landscaping, trees, dividers, or similar noise attenuation and aesthetic features
- be suitably set back in order not to interfere with the improvement or widening of roadways

- 7.3.5 The Town of Crossfield shall require developers within the Downtown and Entrance Area to provide irrevocable security to ensure that any required roadway improvements meet the standards established by the municipality.
- 7.3.6 The Town of Crossfield shall require developers within the Downtown and Entrance Area to contribute through off-site levies to the development of the long-term transportation infrastructure system identified in Figure 8 - Transportation.
- 7.3.7 The Town of Crossfield shall integrate automotive and non-automotive transportation systems and shall require developers to contribute to a comprehensive pedestrian and bicycle pathway system within the Downtown and Entrance Area.
- 7.3.8 The Town of Crossfield should periodically review municipal standards for roadway design within the Downtown and Entrance Area, particularly as they relate to alternative modes of transportation.
- 7.3.9 The Town of Crossfield shall plan and develop dangerous goods route systems and shall adopt and enforce bylaws governing the operation of trucks in order to minimise truck traffic through and within the Downtown and Entrance Area.
- 7.3.10 Development adjacent to Highway 2A and Limit Avenue shall be subject to transportation studies, access restrictions, and requirements for roadway improvements to the satisfaction of Alberta Transportation and the Town of Crossfield.
- 7.3.11 The Town of Crossfield shall collaborate with business, volunteer, and regional partners to develop alternative transportation systems to serve the Downtown and Entrance Area, including:
- bicycle and pedestrian pathway systems within and surrounding the Downtown and Entrance Area
 - taxi, dial-a-bus, and shuttle services
 - commuter bus service
 - passenger rail service
 - a passenger rail station along Railway Street within the Downtown and Entrance Area
- 7.3.12 The Town of Crossfield will consider the incorporation of a mini-roundabout subject to further detailed analysis and consultation with Alberta Transportation.

Figure 8
Transportation



- | | | | |
|---|------------------------------------|---|------------------------------|
|  | Downtown and Entrance ARP Boundary |  | Industrial Road |
|  | Major Road |  | Pedestrian & Bicycle Network |
|  | Primary Collector Road |  | Signal |
|  | Industrial Major Road |  | At Grade Crossing |

8.0 Servicing and Utilities

8.1 Introduction

Growth management, land use planning, and provision of municipal services must be integrated for the Downtown and Entrance Area to function properly in a more sustainable Crossfield. The existing and future water and wastewater infrastructure planned within the Downtown and Entrance Area are identified in Figure 10 - Water Servicing and Figure 11 - Wastewater Servicing.

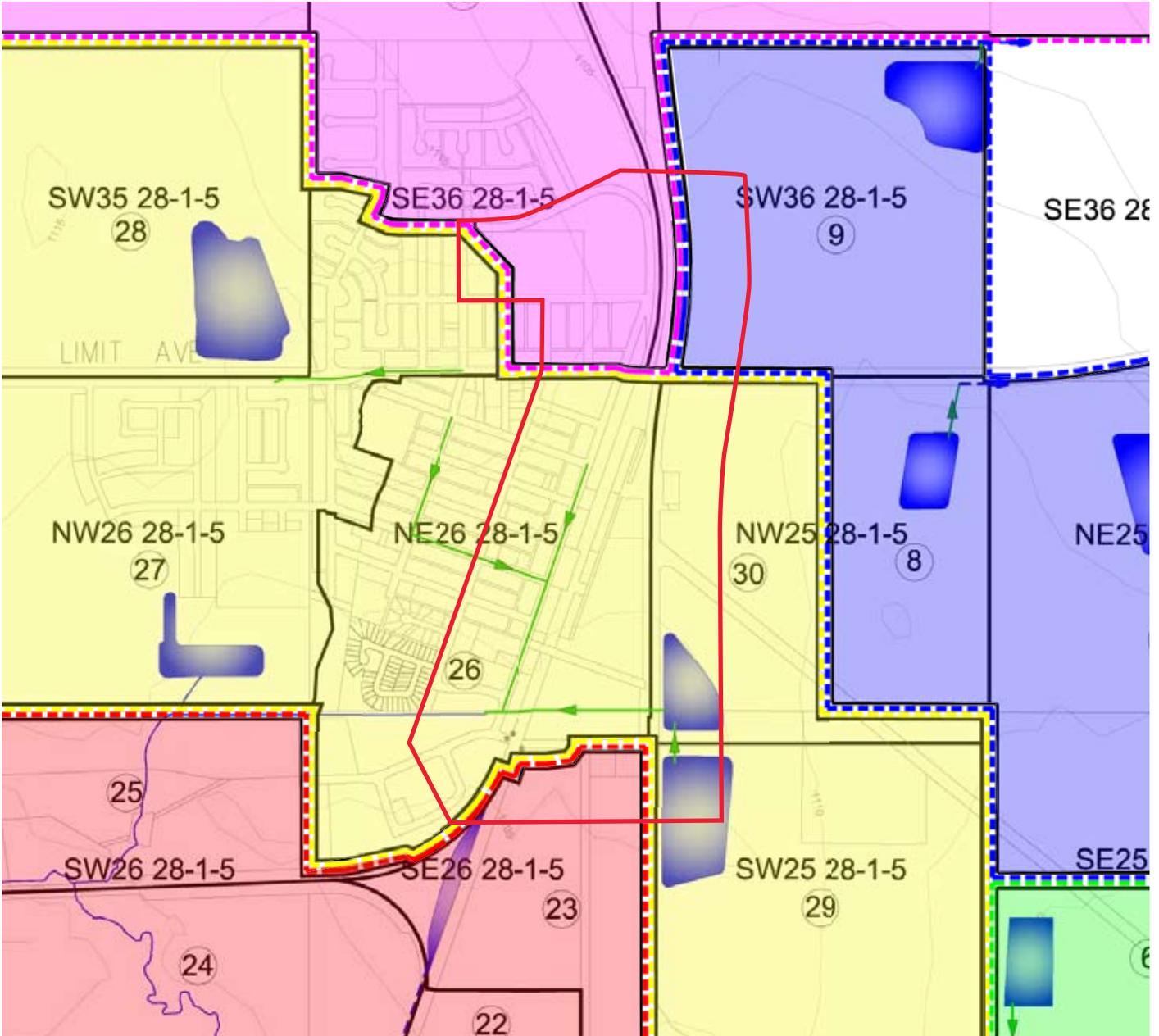
8.2 Objectives

- 8.2.1 To provide for safe, efficient, and cost-effective utility servicing systems within the Downtown and Entrance Area for the benefit of all residents, businesses, employees, and visitors to Crossfield.
- 8.2.2 To sustain the balanced development of utility infrastructure systems within the Downtown and Entrance Area, which will provide environmentally sustainable and reliable services for residents, businesses, employees, and visitors to Crossfield.

8.3 Policies

- 8.3.1 The Town of Crossfield shall coordinate long-range utility servicing plans within the Downtown and Entrance Area in collaboration with Alberta Environment and major regional water and wastewater utilities.
- 8.3.2 The Town of Crossfield shall integrate land use planning with engineering, servicing, and utilities studies within the Downtown and Entrance Area, in order to achieve greater efficiencies and provide better services to the residents and businesses of Crossfield.
- 8.3.3 Upgrades to municipal services in redevelopment areas shall be based upon projected population growth, anticipated intensity of non-residential uses, expected demand for utility services, and the cost implications of such upgrades in those redevelopment areas.
- 8.3.4 All redevelopment within the Downtown and Entrance Area shall be provided with full municipal services, including piped water, piped sewage, stormwater management, natural gas, and franchise utilities (electric power, cable, and telephone).
- 8.3.5 All landowners, developers, or development proponents shall be responsible for the costs of providing adequate water and sewage services, stormwater management facilities, roadways, curbs and sidewalks, and franchise utilities (e.g. gas, power, cable, telephone) to redevelopment areas and sites within the Downtown and Entrance Area.

Figure 9
Master Drainage Concept



- Downtown and Entrance ARP Boundary
- Existing Water Course
- Proposed Storm Sewer Trunk
- - - Proposed Overland Drainage Route
- Proposed Pond Location
- - - South East Industrial Stormwater Management Area
- - - East Commercial Stormwater Management Area
- - - North Residential Stormwater Management Area
- - - South General Stormwater Management Area
- - - Central Downtown Stormwater Management Area
- Sub-Catchment Boundary
- ① Sub-Catchment Reference Number

Figure 10
Water Servicing

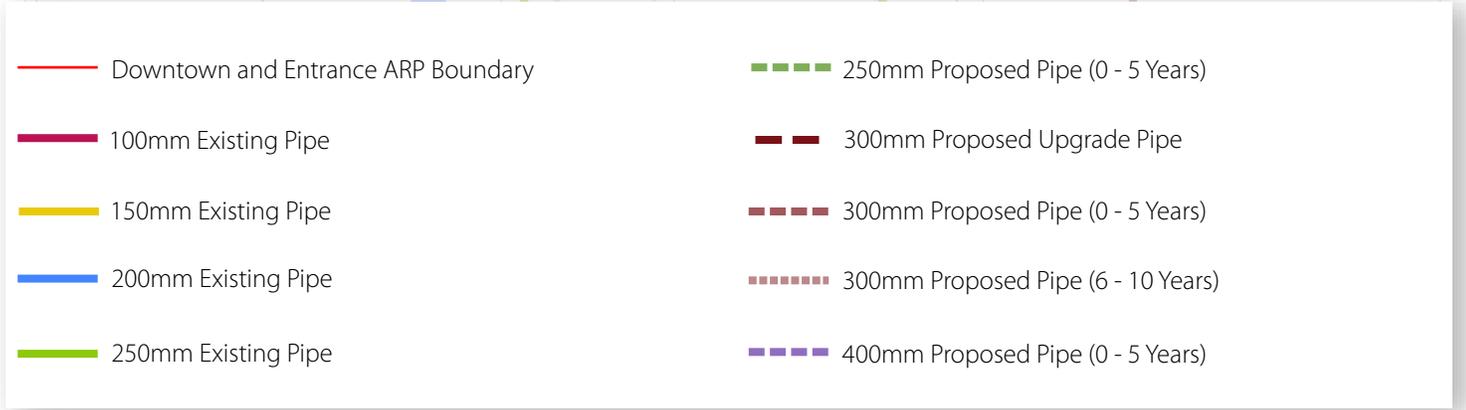
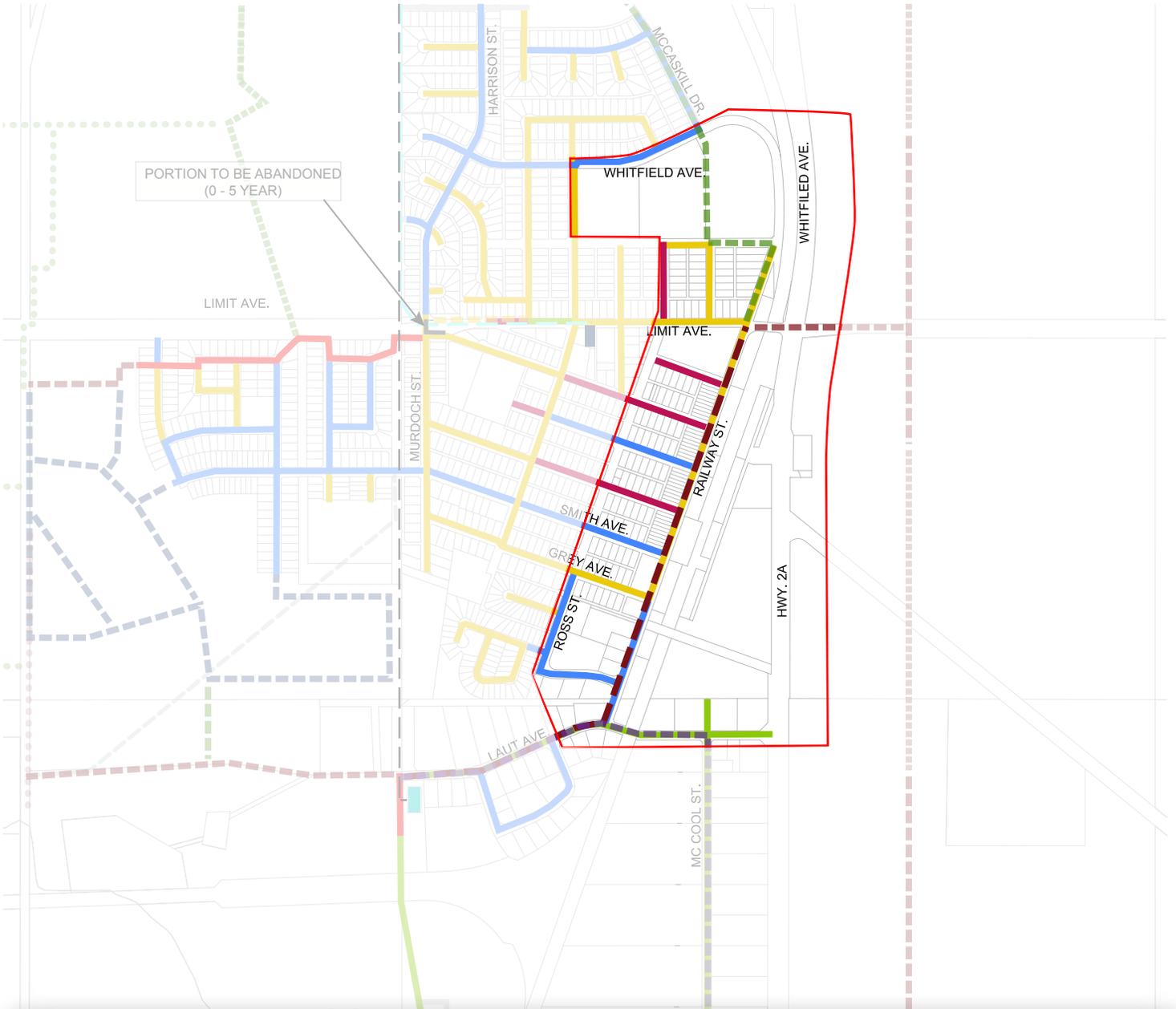


Figure 11
Wastewater Servicing



- Downtown and Entrance ARP Boundary
- 200mm Existing Pipe
- 250mm Existing Pipe
- 300mm Existing Pipe
- 450mm Existing Pipe
- 400mm Drainage
- Forcemain
- - - 5 Year Pipe
- - - 10 Year Pipe
- - - 25 Year Pipe (Full Build)
- Existing Sanitary Liftstation
- Proposed Sanitary Liftstation

2007 Sanitary Usage Statistics

Residential Average Per Capita Production 285 L/per person/day

Industrial Average Per Capita Production 6,276 L/ha/day

Average Sanitary Sewer Inflow 1.51L/s

4 Anaerobic Cells
Volume 13,748m³ (13.7 ML)

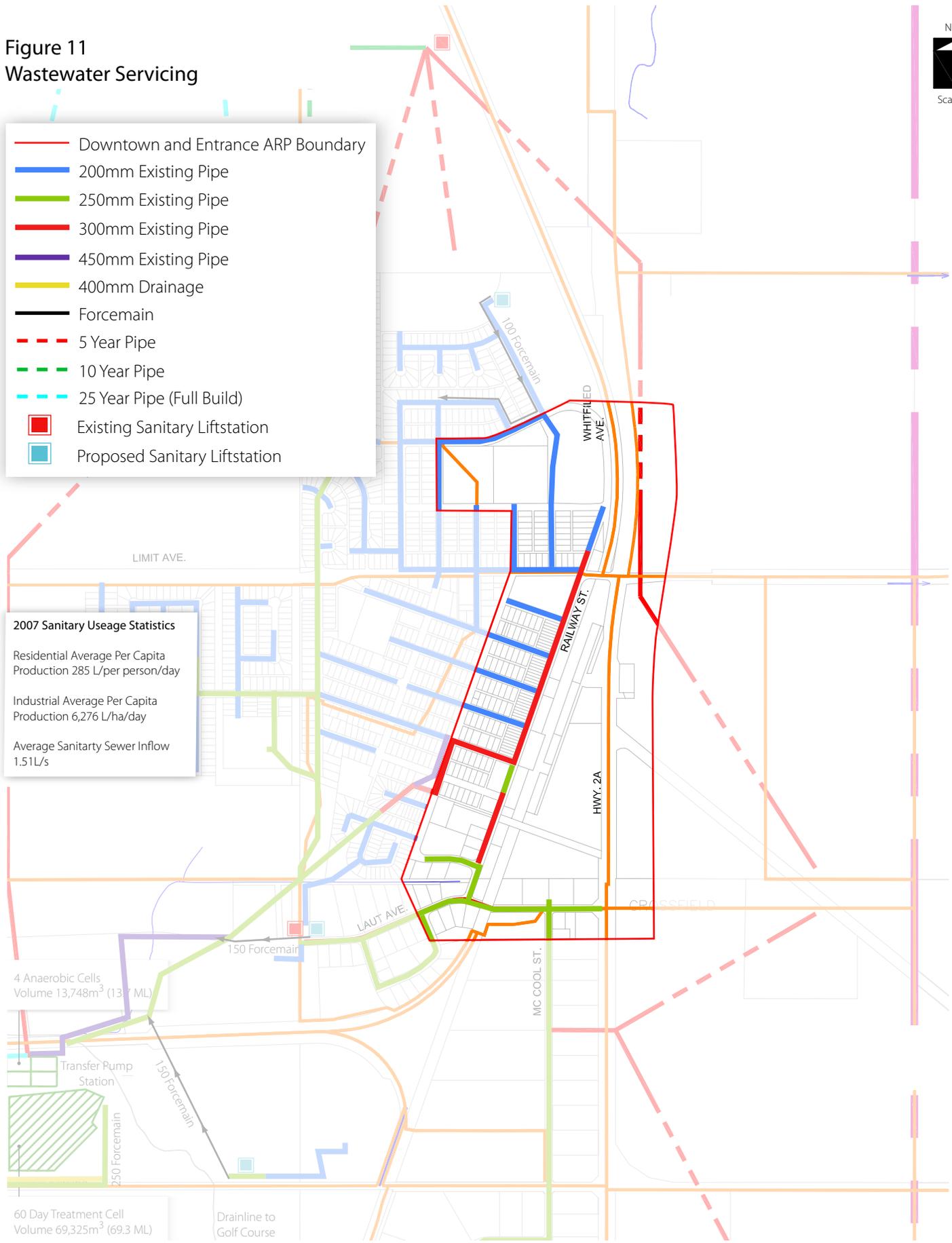
Transfer Pump Station

150 Forcemain

250 Forcemain

60 Day Treatment Cell
Volume 69,325m³ (69.3 ML)

Drainline to Golf Course



- 8.3.6 The Town of Crossfield shall conduct periodic reviews of off-site levy bylaws and redevelopment levy bylaws for the Downtown and Entrance Area to determine whether amendments are required to address the costs of providing municipal servicing infrastructure to redevelopment areas.
- 8.3.7 In addition to off-site development levies and redevelopment levies for the Downtown and Entrance Area, the Town of Crossfield shall investigate the use of alternative fiscal and growth management tools to ensure that the costs and benefits of redevelopment are fairly allocated amongst developers, landowners, residents, businesses, service providers, and the municipality.
- 8.3.8 The Town of Crossfield shall promote residential, commercial, and light industrial water conservation, rainwater harvesting, and the safe use of grey water within the Downtown and Entrance Area.
- 8.3.9 The Town of Crossfield shall require, where required within the Downtown and Entrance Area, the preparation and submission of stormwater management plans prepared by a qualified professional engineer registered in the province of Alberta, which shall demonstrate how the use of stormwater best management practices will reduce post-development run-off rates to pre-development levels.
- 8.3.10 All required stormwater management plans shall include, at a minimum:
- topography of the development lands and surrounding area
 - watershed affected by the development
 - proposed major drainage systems (including the direction of surface drainage)
 - proposed minor drainage systems (including ditches, pipes, and catch basin locations)
 - proposed on-site detention and retention facilities (including locations and sizes)
 - locations of outflow or outfall structures
 - any related modeling or calculation information
- 8.3.11 All required stormwater management plans shall include measures to control the rate and quality of stormwater discharge into significant water bodies, such as the major wetlands and creeks within Crossfield, through the use of stormwater management facilities, avoiding areas of steep and unstable slopes for discharge points, and if feasible, through water quality monitoring.
- 8.3.12 The Town of Crossfield will continue to take the necessary actions to enhance solid waste management and recycling efforts within the Downtown and Entrance Area.
- 8.3.13 The Town of Crossfield shall explore opportunities to use alternative sources of energy such as solar, wind, and geothermal power within the Downtown and Entrance Area.
- 8.3.14 The Town of Crossfield shall conserve energy through the use of low intensity street lamps and other measures that increase energy efficiency in public buildings and facilities within the Downtown and Entrance Area.

9.0 ARP Implementation and Administration

9.1 Introduction

Responsibility for implementation of the ARP rests with Town Council, committees of Council, the Town administration, and through their active involvement in civic affairs, the residents and businesses of Crossfield. Decisions with respect to Land Use Bylaw amendments, subdivisions, and development permits must conform to the policies of the ARP. Capital budgeting also provides the Town of Crossfield with a mechanism for implementing the ARP. Through regular budgeting processes, the Town will be able to make investments in infrastructure and other public improvements that will help to achieve many of the goals and objectives of this ARP.

9.2 Objectives

- 9.2.1 To ensure that all Land Use Bylaw amendments, subdivisions, and development permits are consistent with the ARP.
- 9.2.2 To implement the ARP in a coordinated and cost-effective manner.
- 9.2.3 To promote efficient patterns of development that will enhance the cost-effectiveness of infrastructure expenditures.
- 9.2.4 To provide for efficient, effective, transparent, and participatory planning policy and approval processes.

9.3 Land Use Bylaw

- 9.4.1 The Town of Crossfield Land Use Bylaw, including amendments, shall conform to this ARP and shall be administered to implement the policies of this ARP.
- 9.4.2 The Town of Crossfield Land Use Bylaw shall include land use districts and rules that are consistent with the policies and policy areas of this ARP.
- 9.4.3 The Town of Crossfield Council shall amend the Land Use Bylaw in conformance with this ARP and any amendments to this ARP that Council may adopt.

9.4 Architectural Guidelines and Other Town Policies

- 9.3.13 In addition to the statutory policies of this ARP, the Architectural and Design Guidelines of the ARP are intended to provide a frame of reference for developers and the municipal approving authorities when making redevelopment and approval decisions.
- 9.3.13 In addition to this ARP, Council may authorise the preparation of non-statutory plans and policies, which Council may adopt by resolution, to guide development and redevelopment within the Downtown and Entrance Area.

9.5 ARP Administration

- 9.5.1 Council shall govern and make decisions in accordance with the policies of this ARP by:
- assigning responsibility, within the scope of Council's authority under the Municipal Government Act, to the Town of Crossfield administration, development officer, committees, commissions, and boards for the administration and implementation of the ARP
 - initiating and overseeing the planning programs, budgets, recommendations, and activities necessary to administer and implement the ARP
 - consulting and seeking advice from all interested stakeholders
 - engaging the community in municipal decision-making

- 9.5.2 The Town of Crossfield administration, including the development officer, shall implement the directives of Council in accordance with the policies of this ARP by:
- rendering decisions and approvals as delegated by Council
 - reviewing planning applications and presenting recommendations on those applications to Council, the subdivision approving authority, the municipal planning commission, and the subdivision and development appeal board
 - enforcing the bylaws enacted by Council to implement the ARP
 - ensuring that all programs and activities necessary to administer and implement the ARP are carried out within the budgets set forth by Council
 - consulting and seeking advice from all interested stakeholders
 - effecting the community engagement programs required by Council

10.0 Public Improvement Program

10.1 Introduction

The images of our towns and cities are often defined by the quality of the public realm and the buildings that enclose these spaces. Essentially this includes the boulevards, streets, public squares, parks, public art and street furniture that create a cohesive and uniform environment. The additional benefits of creating aesthetically pleasing surroundings include improved quality of life, a sense of ownership for residents and business owners, and enhanced economic vibrancy.

One of the mandates of the ARP is to provide proposals for public improvement works along Railway Street. These improvements are discussed and illustrated in this section, along with the practical implementation of the improvements and suggested timelines.

10.2 Objectives

- 10.2.1 To provide a realistic public improvement program that can be delivered on time and in conjunction with proposed utilities upgrades.
- 10.2.2 To enhance and improve the public realm within Crossfield through cohesive design elements.
- 10.2.3 To create a walkable downtown core with quality open spaces.
- 10.2.4 To rationalise on-street parking on Railway Street so that it does not dominate the streetscape.

10.3 Utilities Improvements

From time to time, the Town plans for the installation of public utilities, including water, wastewater and sewer connections. The timing of these improvements is subject to capital budget priorities, but it is anticipated that these upgrades will be the trigger for the streetscape improvements outlined in this section. Careful coordination between the Town's engineers and contractors will be required to ensure that optimal efficiency and minimal disruption will result from construction of these improvements.

10.4 Future Rail Connection

The Calgary Regional Partnership has been exploring the possibility of extending rail services to Crossfield by the year 2020. Such service would open up a range of benefits to the Town in terms of improved transport links and visitor numbers. The ARP has explored the possibility of including a passenger rail station as part of the public improvements to the town. The potential locations of this station are illustrated in Figure 12 and include:

1. North of the existing fire hall
 2. Elevator Road
 3. That portion of the NE ¼ 35-28-01 W5M between the CPR tracks and Highway 2A
- 10.4.1 The Town supports the possibility of future rail links to Crossfield and will work with the Calgary Regional Partnership to realise these plans.

Figure 12
Railway Street Strategic Concept



-  Downtown and Entrance ARP Boundary
-  Gateway (entrance feature)
-  Intersection (surface treatment)
-  Civic Public Space
-  Town Hall (to be relocated)

-  Streetscape Improvement Area
-  Open/Green Space Area
-  Pedestrian Crossing Location
-  Retail/Mixed Use Development
-  Potential Passenger Rail Station



Town of Crossfield Downtown and Entrance Area Redevelopment Plan
August 2010

10.5 Streetscape Improvements

A number of streetscape improvements along Railway Street are proposed as part of the ARP. Consultation has taken place with both the residents of Crossfield and the ARP Steering Committee to determine the scope of improvements.

Figures 13 and 14 illustrate the proposed streetscape improvements on Railway Street, which include:

- Introducing a “shared surface” concept onto Railway Street. This means adopting a people-orientated design, whereby design elements (such as reduced signage, removal of steep curbs and differentiation of pedestrian and vehicular lanes through changes in pavement surface treatment) force drivers to use awareness, observation and intelligence to operate safely in a multi-purpose environment
- Textured surface treatment at key intersections and pedestrian crosswalks. The Town will support the use of innovative sustainable materials such as recycled crushed glass
- Wider pedestrian sidewalks along both sides of Railway Street
- The introduction of cycle lanes along Railway Street
- Reduced dimensions for the vehicular right-of-way along Railway Street

10.5.1 The Town supports the streetscape improvements as outlined in the ARP and will issue Requests for Proposals to qualified contractors for the installation and associated costs for delivering the streetscape improvements.

10.5.2 The Town will support locally sourced and sustainable materials, including permeable paving materials.



Examples of a shared surface street design

Figure 13
Conceptual Streetscape Improvements



Access from Limit Avenue via potential mini-roundabout



Firehall



Town Hall (to be relocated)

Proposed Street Trees along western edge of Railway Street

Osler Avenue

Pedestrian Crossing

Hammond Avenue

Attract investors to ensure vacant retail units become occupied

Intersection (tactile surface treatment)

Shorty's Garage

Ensure strong building frontage along Railway Street

Smith Avenue

Existing Trees Retained

Access from Limit Avenue via potential mini-roundabout

Canadian Pacific Railway

Bigway Foods Supermarket

New grassed areas until further development

Desired landmark building at key intersection

Future development

Mountain View Credit Union

Adaptable Civic Space (exact location to be determined)

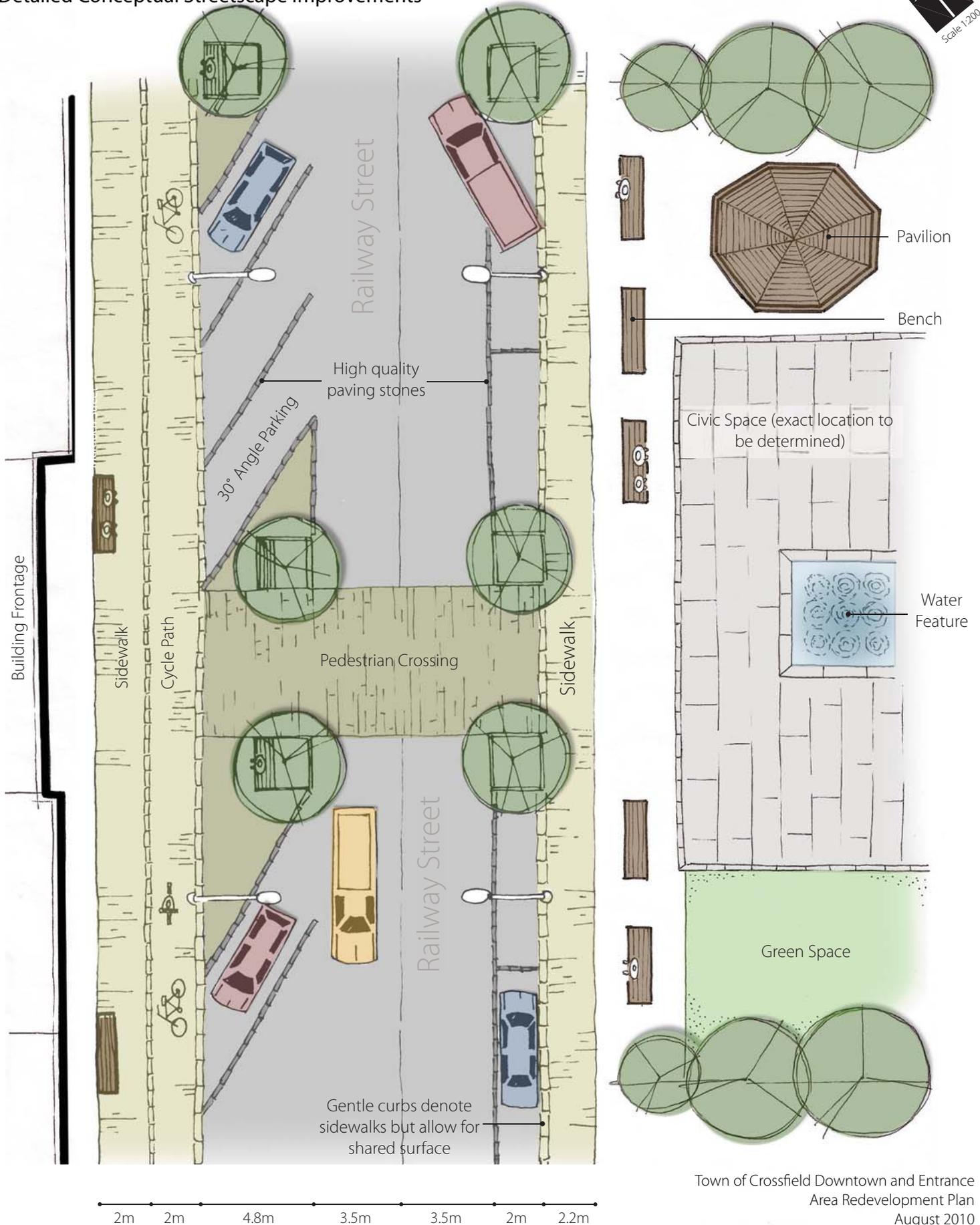
Nanton Avenue

Potential future surface parking lot (pending resolution of contamination issues)

Screening along Railway

Canadian Pacific Railway

Figure 14
Detailed Conceptual Streetscape Improvements



Town of Crossfield Downtown and Entrance
Area Redevelopment Plan
August 2010

Drawing No. 423b-04b

10.6 Civic Space

- 10.6.1 The Town supports the proposal to include a new civic public space within the downtown area of Crossfield and will investigate the potential locations of this space. The town will pursue conversations with landowners to discuss the possibility of acquiring land.
- 10.6.2 The Town supports proposals for a new gazebo/bandstand within the Railway Street Redevelopment Area, and will explore ways forward to ensure this is accomplished.

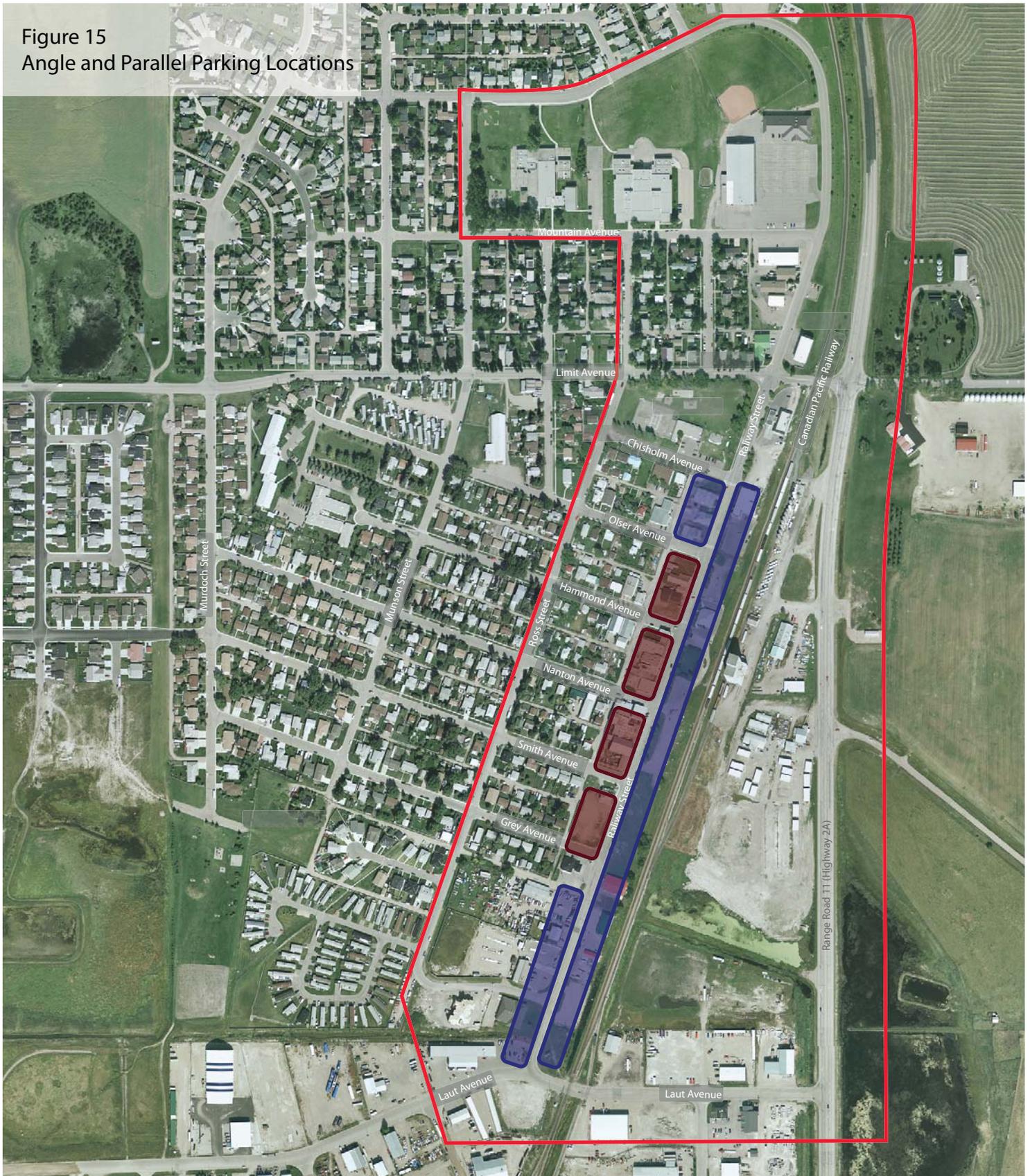
10.7 Parking Rationalisation

Extensive discussions have been held within the ARP Steering Committee regarding the reconfiguration of parking along Railway Street. Proposals were originally put forward introducing parallel parking along both sides of Railway Street. This would require a surface parking lot to offset the reduced number of street spaces, although it would allow for a great proportion of the street width to be dedicated to pedestrian and cycle movement. Nonetheless, it was determined that in the absence of an immediate surface parking lot, angled parking should be retained along the more popular blocks within Railway Street.

Figure 15 on the following page illustrates where angle parking will be retained and parallel parking introduced.

Policy 5.4.6 outlines the ARP policy for parking rationalisation.

Figure 15
Angle and Parallel Parking Locations



-  Downtown and Entrance ARP Boundary
-  Angled Parking at 30°
-  Parallel Parking



10.8 Street Furniture

As part of the first ARP Open House event, a number of proposals were suggested for the style of street furniture to be adopted in Crossfield. These were: traditional; themed; and contemporary. The ARP Steering Committee agreed that the proposals should be forward thinking and that the Town should plan for the future residents of Crossfield, so a suggested alternative “green” theme was supported. This includes any sustainable initiatives or products that utilise renewable energy. However, since support was also received for the traditional theme, careful consideration would need to be given to ensure that the heritage of Crossfield is respected. It is hoped that with further research, sustainable furniture that has a more traditional aesthetic can be sourced. The street furniture that will be replaced includes (but is not limited to):

- New solar powered street lighting
- New solar powered compacting garbage/recycling containers
- New benches
- New planting (boulevard trees and seasonal hanging baskets)
- New bollards along the east side of Railway Street between the pavement and the vacant land

10.8.1 New street furniture shall be robust, durable and in keeping with the sustainable theme, whilst respecting the heritage of Crossfield.

10.8.2 The Town will support street furniture that is constructed of locally sourced and recycled materials.

10.8.3 Existing trees shall be retained unless their removal can be mitigated elsewhere within Railway Street.

10.8.4 The Town supports the suggested “green” themed street furniture upgrades as outlined in the ARP and will issue Requests for Proposals to qualified contractors for the installation

and associated costs for delivering these improvements.

The images below illustrate some of the street furniture that could be incorporated along Railway Street.



- 1 Solar powered street light
- 2 Bench made from recycled plastic
- 3 Solar powered motion activated crosswalk light
- 4 Bollards made from recycled composite
- 5 Solar powered compacting waste container

10.9 Public Art

As part of its policy of securing a high quality built environment, the Town of Crossfield is promoting a plan whereby developers devote a percentage of development costs to the installation of a work or works of public art.

Artwork adds distinctiveness to development and can raise commercial appeal. In cases where development costs total \$800,000 or more, developers are asked to devote up to 1% of those costs to a work of art to be displayed within the development and visible to the public.

The Town believes that art assists in providing cultural identity, can add character and quality to the built environment and can provide commercial appeal and identity.

- 10.9.1 Developers shall be encouraged by the Town to either:
- set aside up to 1% of the total development cost of projects costing \$800,000 (excluding the sale of land) or more for provision of a work of art
 - discuss potential tax incentives for the provision of a work of art

The artwork should be integrated as an essential part of the development. It could be sited externally or internally, though it should be accessible or visible to the general public or, where this is not possible, to the client public.



Public art gives a sense of ownership to the community and provides a focal point for prominent sites

The form of the artwork will be a matter for discussion and agreement with the Town. It could be a sculpture, painting, mosaic, tapestry or an individually designed piece of street furniture.

10.10 Implementation

A key objective for the implementation of this ARP is to ensure that its proposals and vision are taken forward systematically and effectively in the future and that the area benefits from steady and continuous regeneration.

This will be assisted by setting up structures and partnerships to drive forward the policies and the intentions identified in this document.

This section concerns how that can be achieved. It will be more fully developed following consultation with key stakeholders, landowners and the community at large. The proposed public realm improvements will be implemented by a range of methods over a long period. However, these are not intended to be prescriptive. The circumstances of each improvement will determine how it is implemented. It should be noted that one of the primary purposes of this document is to ensure coherence of design and quality, despite the diverse methods of delivery.

There are clearly practical limitations, including the physical limitations of the existing built environment, but also financial implications to the Town and other stakeholders for what is proposed. The Town is, and will be seen as, a major figurehead in encouraging and facilitating development. The Council anticipates funds

being made available for the streetscape improvements initially, to help foster a climate of renewal and to demonstrate commitment.

Potential redevelopment incentives could include:

- Fee reductions
- Permitting assistance
- Off-site improvements
- Façade improvement grants
- Low-cost financing incentives
- Property tax credits

10.10.1 The Town of Crossfield will coordinate action to implement this plan, by setting up appropriate mechanisms and partnerships to deliver its objectives. These will include partnerships with the Crossfield Chamber of Commerce, individual business operators, and the Crossfield Sustainability Advisory Board.

10.10.2 The Town will monitor the progress on implementing the ARP and, if progress is slow or obstructed, will take measures to bring it to fruition.

10.11 Phasing and Timing

A realistic phasing program has been established to promote appropriate development in a logical order. Clearly, market demand economic conditions will have an influence on the phasing of development, although what is proposed here is the anticipated order of development.

Potential phasing options as outlined in Table 1 offer practical solutions in the short to long term in a coordinated and structured way, working with the existing stakeholders within the town.

Table 1 Potential Phasing Options

| Phase | Type of Improvement | Timeline | Responsibility/Actions |
|-------|--|-------------|--|
| 1 | Utilities upgrades | Short term | Town's engineers (D.A. Watt) |
| 1 | Streetscape/ revitalisation improvements | Short term | Issue RFP to landscape architects/ contractors (Town) |
| 1 | Parking rationalisation | Short term | Issue RFP to landscape architects/ contractors to reconfigure the parking layout/surface treatment (Town) |
| 1 | Street furniture | Short term | Issue RFP to landscape architects for the program of works and costs for installation of street furniture (Town) |
| 1 | Public art | Short term | Apply ARP policy for new development to ensure public art is delivered |
| 2 | New amenity space | Medium term | Town to liaise with landowners about potential sites located centrally on Railway Street |
| 3 | Passenger rail station/connection | Long term | Coordination between the Town and Calgary Regional Partnership |

Short term - committed projects/planning approvals likely to be completed within the next 3 years

Medium term – up to 10 years to complete

Long term - potential projects that may take up to 20 years to bring to completion

In the main, proposals are linked to and dependent upon private sector funding, through the delivery of redevelopment plans. However, the Town is committed to pursuing any additional funding necessary to deliver the proposals of the plan.

The improvements required to the public realm are extensive and will require considerable further work to assess feasibility and the optimal means and timing of implementation.

10.12 Next Steps

Crossfield Downtown and Entrance Area ARP is the means for enhancing Crossfield to make it a flourishing place where people choose to live, work and visit. The ARP aims to realise the exceptional opportunity provided and to reflect a shared vision for the study area that can create not only a distinctive identity and sense of place, but also provide a robust framework for investment and decision making.

The ARP will be used to guide the improvement program. The next steps will be to undertake detailed feasibility and design work on proposals and then draw up implementation programs.

GLOSSARY

Alberta Municipal Government Act (MGA) means the Municipal Government Act, Statutes of Alberta 1994, Chapter M-26.1 as amended from time to time.

Apartment building means a building with a number of self-contained housing units.

Architectural and design guidelines means guidelines intended to ensure design continuity in the context of the local community, neighbourhood, or street, while maintaining a number of architectural styles to offer variety and unique identity.

Area Redevelopment Plan (ARP) means a statutory plan, adopted by Bylaw, that outlines proposed redevelopment for a specified area, and which sets forth municipal policies:

- to preserve or improve land or buildings in the ARP area
- to rehabilitate, remove, construct, or replace buildings in the ARP area
- to establish, improve, or relocate roads, public utilities or other services
- to establish land use and architectural guidelines for development in the ARP area
- to establish means such as redevelopment levies to pay for public improvements in the benefitting ARP area
- to facilitate any other development in the ARP area

Area Structure Plan (ASP) means a statutory plan, adopted by Bylaw, that provides a policy framework for the evaluation of proposals for outline plans, land use redesignation, subdivision and development of a specified area of land in the municipality.

Best Management Practices for stormwater management means a method by which adverse stormwater impacts from development or redevelopment, including but not limited to the release of pollutants into water, are controlled through the application of schedules of activities, prohibition of practices, maintenance procedures, structural protocols, and managerial practices.

Big box retail (also known as a supercenter, superstore, or megastore) is a physically large (often single storey) retail establishment, usually part of a chain and aimed at vehicle users with a large expanse of parking located at the front of the store.

Business taxes means taxes that businesses pay to local governments, not including any property taxes that business property owners may be liable for paying to local governments.

Calgary Regional Partnership means an association of Calgary and surrounding municipalities that cooperate in making growth and development decisions.⁷

Capital budgeting means a method for evaluating investment proposals to determine whether they are financially sound, and for allocating limited capital resources to the most desirable proposals.

Citizen engagement means an interactive process of deliberation among citizens, to contribute meaningfully to public policy decisions in a transparent and accountable manner.

Commercial development means development that includes activities intended for either the sale of goods or the provision of services, or both.

Community sustainability means the condition of a community in which the diverse needs and aspirations of its existing and future residents are met, in a manner that is sensitive to the natural environment, that manages long term resources, and that contributes to a high quality of life.

Council means a body of elected officials who exercise duties and powers as legislated by the province. In this document, "council" refers to the Crossfield Town Council.

Council committee means a committee that may consist entirely of councillors, a combination of councillors and other persons, or subject to section 154(2) of the MGA, entirely of persons who are not councillors, who are officially delegated to perform functions such as researching, considering, reporting, or acting on municipal matters.

Crime Prevention Through Environmental Design (CPTED) means a multi-disciplinary approach to deterring criminal behaviour through environmental design, which relies upon the ability to influence offender decisions that precede criminal acts, mostly within the built environment.

Crossfield Sustainability Plan means the non-statutory plan adopted by the Town of Crossfield Council in 2009, as may be amended from time to time, which is based upon extensive community engagement and which includes the expression of a community vision, sustainability principles, and action plans.

Developed (Development) means:

- any excavation or stockpile and the creation of either of them
- a building or an addition to it
- the replacement, repair, or construction of a building
- the placement of a building in, on, over, or under land
- a change of use of land or a building
- a change in the intensity of use of land or a building

Development constraints means conditions that hamper development in a particular area.

Development officer means an official charged with administering the applicable bylaw and deciding on development permit applications.

Development permit means a document that includes approved site and building development plans illustrating land use, landscaping, built form, intensity of use, and appearance of the site and buildings, as well as conditions of development approval.

Emergency services means police services (provided in Crossfield by the RCMP), fire department (Crossfield Fire Department) and EMS (provided by the Government of Alberta).

Environmental reserve means reserve lands to be owned by a municipality, which the municipality may require a subdivision applicant to provide, for lands defined in the MGA as:

- a swamp, gully, ravine, coulee or natural drainage course,
- land that is subject to flooding or is, in the opinion of the subdivision authority, unstable, or
- a strip of land, not less than 6 metres in width, abutting the bed and shore of any lake, river, stream or other body of water for the purpose of
 - » preventing pollution, or
 - » providing public access to and beside the bed and shore

Extended care facilities means facilities designed: (1) to aid patients who have been hospitalised but no longer need the full complement of hospital services or (2) to serve those who are not acutely ill but who require skilled care.

Fiscal responsibility means the requirement for municipalities set forth in the MGA that municipalities must balance budgets and may not carry deficits.

Geotechnical means aspects of or related to the soil and bedrock, especially as they may affect foundations and earthworks.

Geothermal power means the harnessing of heat from beneath the earth's surface to generate electricity with virtually no emissions, using subterranean steam or hot water to turn turbines that produce electricity.

Green roof means a roof of a building that is partially or completely covered with vegetation and a growing medium. It may also include additional layers such as a root barrier and drainage and irrigation systems.

Green wall means a wall, either free-standing or part of a building, that is partially or completely covered with vegetation, and in some cases, soil or an inorganic growing medium.

Guidelines means statements of planning intent that are more detailed than policies, but not as strict as rules and regulations.

Hydrogeological means the distribution and movement of groundwater in the soil and rocks of the earth's crust (commonly in aquifers).

Impact analysis means the determination of the effect that a change to a function or component will have to other functions or components as well as to other systems.

Industrial development means development including manufacturing, processing, fabrication, storage, distribution, or other intensive or extensive business activities that provide significant employment and economic development for the community.

Infrastructure means the services and facilities for which the municipality has capital investment and maintenance responsibilities, including roadways, sidewalks, bridges, street lights and traffic signals,

transit buses, solid waste management systems, potable water distribution systems, storm sewers, sanitary sewers, sports fields, playgrounds, arenas, pools, police and emergency response stations, vehicles and equipment, civic buildings, parks, boulevard trees and computer and telecommunications equipment.

Land Use Bylaw means a bylaw of the municipality passed by Council as a Land Use Bylaw pursuant to the provisions of the Municipal Government Act and intended to control and regulate the use and development of land and buildings within the municipality.

Land use redesignation means a statutory decision of a municipal Council, often referred to as re-zoning, which legally changes the acceptable uses for specific parcels of land.

LEED- (Leadership in Energy and Environmental Design) means a system to categorise the level of environmentally sustainable construction in buildings.

Light rail transit (LRT) means a transit mode typified by an electric railway with medium volume traffic capacity compared to heavy rail, which is characterised by passenger rail cars operating on fixed rails in a shared or exclusive right-of-way.

Local improvement bylaw means a municipal bylaw approved in accordance with Division 7 of the MGA for the purpose of implementing local improvements and collecting local improvement taxes to fund such local improvements.

Master drainage plan means a plan to address the current and future drainage needs of an area, having due regard for water management goals that may be established in studies at a watershed scale.

Municipal Development Plan (MDP) means the principal statutory land use plan for the entire municipality, adopted by Council, in accordance with the provisions of the Municipal Government Act.

Municipal planning commission means a commission that Council establishes by bylaw:

- to serve as a development authority
- to advise and assist Council with regard to the planning of orderly and economic development within the municipality
- to ensure that proposed developments shall be approved in accordance with the purpose, scope, or intent of the MDP, IDP, ASPs, ARPs, Land Use Bylaw, and any applicable non-statutory plans or policies approved by Council

Municipal reserve, municipal and school reserve, and school reserve, means reserve lands to be owned by a municipality, which the municipality may require a subdivision applicant to provide, up to ten percent of the developable area, for park, recreation, or school authority purposes (designated MR for municipal reserve, MSR for municipal and school reserve, or SR school reserve).

Natural environment means self-sustaining areas with native vegetation, water, or natural features.

Non-government organisation means a legally constituted organisation that operates independently from any level of government.

Non-statutory plans means plans that are similar in scope to statutory plans, but which are approved through Council resolutions and may include conceptual schemes, land use policies, architectural guidelines, and policy statements.

Off-site levy means a development levy that a Council may impose by bylaw in accordance with the MGA to be used to pay for capital costs, such as water storage, treatment, or supply facilities, sanitary sewage facilities, storm sewer, or roads, which may not be located on the subject development site but which would directly or indirectly benefit the subject development.

Physical planning means a form of urban land use planning which attempts to achieve an optimal spatial coordination of different human activities for the enhancement of the quality of life.

Planning approvals means the exercise of municipal authority to approve land use redesignations, subdivisions, and development permits.

Policy means an official plan of action adopted by an individual or group, which for land use plans adopted by municipalities in Alberta can be distinguished as either statutory plans (Municipal Development Plans, Area Structure Plans, or Intermunicipal Development Plans) or non-statutory plans.

Public art means works of art in any media that has been planned and executed with the specific intention of being sited or staged in the physical public domain, usually outside and accessible to all.

Public health and safety means the overall well-being of the people in a community.

Redevelopment levy means a levy that may be imposed on an applicant for a development permit in a redevelopment area, in accordance with an ARP, municipal bylaws, and the MGA.

Regional commercial development means significant office and retail commercial development outside of predominantly residential neighbourhoods, which cater to large trade areas.

Residential development means development that includes all manner of dwellings and associated uses intended for habitation by persons.

Rules and regulations means clear, unambiguous standards (such as maximum and minimum standards) laid out in bylaws, such as a Land Use Bylaw.

School Division means a geographic division over which a school board has jurisdiction, in accordance with the provisions of the Alberta School Act and the Municipal Government Act.

Semi-detached housing means a pair of houses built side by side as units sharing a dividing partition or common wall between the two adjoining buildings.

Serviced land means land that has been serviced with municipal sewer or water services.

Single-detached housing means a free-standing residential building, generally built on a lot larger than the structure itself, adding an area surrounding the house, which is commonly called a yard.

Social development means a commitment to individual well-being and volunteerism, and the opportunity for citizens to determine their own needs and to influence decisions which affect them.

Statutory plans means plans required or enabled by the MGA that are adopted by municipal Councils through public hearings and which include MDPs, IDPs, ASPs, and ARPs.

Subdivision means the creation or separation of new titled parcels of land from an existing parcel of land, which may sometimes be referred to as the parent parcel.

Subdivision and development appeal board means a body appointed by a municipal Council in accordance with the MGA to hear appeals of decisions made by a development officer or subdivision authority.

Subdivision and development regulations means regulations promulgated by the Lieutenant Governor in Council under section 694(1) of the Alberta MGA.

Subdivision approving authority means a body or person legally empowered by a municipal Council to make subdivision decisions.

Sustainable development means development that meets the needs of the present without compromising the ability of future generations to meet their own needs.

Tax increment financing means a financing tool used by municipalities and other development authorities to finance the costs of developments such as housing or public improvements, in designated underdeveloped areas, by which the municipality can provide initial funding for investments by borrowing against the additional tax revenue expected after completion of the improvements.

Townhouses means a variety of residential buildings where individual houses lining a street share adjacent walls in common and have continuous stretches of roof.

Traffic impact assessment means a tool used to analyse traffic generated by proposed developments with either new access or increased use of existing access points.

Universal Design means design that makes a building accessible to as many people as possible, including but not limited to older people and those with physical handicaps.

Utilities means either (1) municipal and regional utilities such as water and sanitary sewer or (2) "shallow" utilities such as gas, telephone and electric.

Wetland means an area of land, including a swamp, marsh, or bog, the soil of which is saturated with moisture either permanently or seasonally and which may also be covered partially or completely by shallow pools of water.

Adopted by the Town of Crossfield Council September 21, 2010

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